



**Town of Paradise**

**Community Development Block Grant Disaster Recovery – Infrastructure**

**Action Plan**

**Amendment 2**

January 9, 2024

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## 1. Amendment 2 Summary

The Town of Paradise is issuing Action Plan Amendment 2 to incorporate developments relating to Roe Road Extension Phase 2 being recommended for funding under the California Transportation Commission's Local Transportation Climate Adaptation Program (LTCAP) Cycle 1. These LTCAP funds provide a significant portion of the project's funding picture, however, additional funds are needed in the development phase (\$4M) and construction phase (\$28M). CDBG-DR Infrastructure funds are now recommended for the development phases while construction funding will be pursued elsewhere. This Amendment reduces the Oliver Curve Pathway Phase 1 Project CDBG-DR budget by \$10M to free up funding to account for the \$4.2M in Roe Rd Extension Phase 2 costs as well as anticipated cost increases to three other projects due to inflation and other changing economic factors. The Town of Paradise will leverage the remaining \$3.1M in CDBG-DR funds on the Oliver Curve Project towards an Active Transportation Program Cycle 7 application scheduled for spring 2024.

More information on the specific changes to the Amendment can be found in Sections 10-14 of this document.

## 2. Purpose

The Town of Paradise CDBG-DR Infrastructure Action Plan aims to document the context, approach and overview of potential use of allocated funds. Future revised Action Plans will be presented:

- Annually during budget processing, and
- As needed when critical issues develop requiring Council action such as changes to the Recommended Project List

## 3. CDBG-DR General

On January 27, 2020, United States Department of Housing and Urban Development (HUD) published Federal Register Notice 85 FR 4681 allocating \$1,017,399,000 in CDBG-DR funding, related to the Federal Emergency Management Agency (FEMA) Major Disaster Declarations DR-4382 from July to September 2018 and DR-4407 in November 2018. The California Department of Housing and Community Development (HCD) is the grantee responsible for administering the CDBG-DR funds allocated to the State of California. CDBG-DR supports the State of California's unmet recovery needs related to the Federal Emergency Management Agency (FEMA) Major Disaster Declarations DR- 4382 from July to September 2018 and DR-4407 in November 2018.

Recognizing unmet infrastructure recovery needs, related to DR-4382 from July to September 2018 and DR-4407 in November 2018, HCD allocated \$317,428,488 of the CDBG-DR funding to the Disaster Recovery Infrastructure Program (DR-Infrastructure). DR-Infrastructure projects are funded to assist with meeting the unmet infrastructure needs of local communities. This program provides funding for FEMA Public Assistance (PA) match projects, FEMA Hazard Mitigation Grant Program (HMGP) match projects, other non-FEMA match projects, and stand-alone projects identified by local communities impacted by DR-4382 or DR-4407. HUD requires that 70% of the total grant-wide funds are spent on activities that meet the low- and moderate-income (LMI) national objective and 80% of grant funds are expended in the HUD-identified most impacted and distressed (MID) areas. The MID areas for the 2018 DR-Infrastructure Program include the entire counties of Butte, Shasta, Los Angeles and Lake; the Town of Paradise is MID eligible within Butte County.

## 4. CDBG-DR Infrastructure Program Activities

The DR-Infrastructure Program utilizes CDBG-DR funds to address two needs of Eligible Applicants: 1.) providing support to disaster-impacted units of local government and other eligible entities with payment of

their non-federal local cost share (FEMA Public Assistance (PA) Match, FEMA HMGP Match, and other federal grants) requirement so that they can access other disaster recovery resources without incurring an unexpected financial burden to address recovery needs; and 2.) providing funds to units of local government to develop “Stand- Alone” infrastructure projects, which can be funded with up to 100% CDBG-DR funding, that are necessary to address unmet disaster recovery needs from impacts tied to DR- 4382 or DR-4407. To address the needs of Eligible Applicants described in this paragraph, the DR-Infrastructure Program funds are used for the following project types:

- The non-federal local cost share match on disaster-related federal grants (FEMA and other federal grants); and
- Non-match, Stand-Alone Infrastructure projects that address identified unmet 2018 disaster recovery needs and increase the resilience of cities and counties.

#### a. FEMA PA Match

The FEMA Public Assistance (PA) Grant Program supports communities’ recovery from major disasters by providing them with grant assistance for debris removal, life-saving emergency protective measures, and restoring public infrastructure. FEMA provides grants to the State and its sub-applicants (cities, counties, school districts, etc.) to address the long-term rebuilding, recovery, and resiliency needs of the communities. Projects that are approved for FEMA PA Match in certain FEMA Categories are eligible for non- federal share, local match funding under the DR-Infrastructure Program. See Section 2.4 for a full description of eligible FEMA PA Match projects, activities, and compliance.

#### b. FEMA HMGP Match

FEMA Hazard Mitigation Grant Program (HMGP) activities are designed to help communities implement hazard mitigation measures following a Presidential Major Disaster Declaration in the areas of the state, tribe, or territory requested by the Governor or Tribal Executive. The key purpose of HMGP is to enact mitigation measures that reduce the risk of loss of life and property from future disasters. Projects that are approved for FEMA HMGP Match are eligible for non-federal, local match funding under DR-Infrastructure. See Section 2.5 for a full description of eligible FEMA HMGP Match projects, activities, and compliance.

#### c. Other Non-Federal Match

HCD may fund the local portion of the non-federal share for other federally grant- funded infrastructure projects with a tie-back to the DR-4382 or DR-4407 disaster events. Grants include, but are not limited to, projects funded by the Federal Highway Administration (FHWA) and the United States Department of Agriculture (USDA).

#### d. Stand-Alone Infrastructure

Non-match, Stand-Alone CDBG-DR eligible infrastructure projects with a tie-back to the 2018 disasters that can be funded with up to 100 percent of CDBG-DR funding are eligible for the DR-Infrastructure Program. These non-match, Stand-Alone Infrastructure projects are critical to address identified unmet disaster recovery needs and increase the resilience of cities and counties that are not funded by other federal recovery programs. All Stand-Alone Infrastructure projects require an environmental review be completed by the Subrecipient. See Section 2.6 for a full description of eligible Stand-Alone Infrastructure projects, activities, and compliance.

## 5. CDBG-DR Infrastructure Program Requirements

### a. Tie-back to the Disasters

All projects funded through the DR Infrastructure Program must in some way respond to a direct impact from the following federally declared disasters:

- DR-4382 California Wildfires and High Winds (July 23, 2018 – September 19, 2018)
- DR-4407 California Wildfires (November 08, 2018 – November 25, 2018)

The FEMA PA program requires clear documentation showing a direct disaster related impact as a prerequisite for entry into the program. Only after an impact threshold has been met does FEMA consider making disaster funds available to applicants. It is assumed that if the potential applicant received funding and support through the FEMA PA program, the project has a tie to one of the declared disasters. For FEMA PA Match projects, the tie to the 2018 disasters is documented by FEMA's approval of the Project Worksheet.

For Stand-Alone projects, HMGP Match projects, and other Federal match projects, the tie to the 2018 disasters is documented as part of the Notice of Intent (NOI) and application processes and stored in the program file of Grants Network. Documentation that shows a tie to the disaster can include, but is not limited to, a damage estimate prepared by city/county staff or contractor, reconstruction estimates for physical losses by engineers or other similar professionals, insurance claims, or photographic evidence of the physical impact of the disaster on the project with clear dates and timeline.

Eligible Applicants may submit indirect tie backs to the disaster that are a result of population shifts due to DR-4382 and DR-4407. For such indirect tie backs, Eligible Applicants are required to use one of two options. Option one is to use publicly available data from a government entity or research center acceptable to HCD to determine whether the jurisdiction experienced a sustained population increase, of at least 24 months, reasonably attributable to an influx of 2018 disaster survivors. Option two is for Eligible Applicants to provide alternative data documenting a sustained population increase, of at least 24 months, reasonably attributable to an influx of 2018 disaster survivors, using a survey methodology acceptable to HCD (i.e., reasonably designed to produce an accurate estimate).

For all projects, HCD reviews how each project supports the overall housing recovery from DR-4382 or DR-4407. Projects that support overall housing recovery are infrastructure projects that enable the recovery of residential areas by meeting transportation needs, restoring essential utilities, and addressing other public infrastructure and facility needs. Examples of infrastructure projects that support overall housing recovery include, but are not limited to public roads, school facilities, stormwater drainage improvements, potable water, sanitary sewer, electric and gas utilities, wastewater treatment facilities, parks, and other public facilities that are important publicly owned assets.

### b. Meeting a National Objective

In accordance with 24 CFR 570.208, all CDBG-DR funded activities must meet a national objective as required under 24 CFR 570.200(a)(2). Under section 101(c) of the authorizing Act (42 U.S.C. 5301) the CDBG program must ensure that the funded activity meets one of the named national objectives. The two qualifying national objectives are:

- Benefiting low- and moderate-income (LMI) persons; and
- Meeting an urgent need.

National Objective Category	Subcategory	Required Documentation
LMI Benefit	LMI Area Benefit	Boundaries of service area of activity/project Census data including the persons and percentage LMI Evidence area is primarily residential Survey documentation (if applicable)
LMI Benefit	LMI Limited Clientele	Documentation that the beneficiaries are or are presumed to be LMI by category (e.g. senior housing, homeless shelters, etc.).
LMI Benefit	LMI Housing	If applicable, income document for all household members 18 years of age and older.
Urgent Need	Urgent Need	Resolution from Subrecipient's city council, county board, or similar governing body stating that no other funds are available for the proposed project.

The DR-Infrastructure Program must demonstrate that funded activities meet one of the two National Objectives. The types of records to be maintained for each CDBG-DR funded project depends on the National Objective category for which it qualifies. For all DR-Infrastructure Program projects, the final determination of the National Objective is completed during the application process based on HUD guidance. Eligible Applicants also need to provide preliminary National Objective documentation as part of the NOI process to support the Priority Level of the project.

Determining the service area of an activity involves consideration of the nature, location, and accessibility of each activity, and the information contained within the project's description and scope of work (contained in the Project Worksheet for FEMA PA projects). Service areas are based on the beneficiaries of recovery activities and/or types of facilities (e.g. work on roads and bridges, repair/replacement of public utilities, etc.); geographic features (e.g. locations of highway, rivers, hillsides, etc.); and local population characteristics (e.g. population size and density). Wherever possible, HCD leverages pre- defined service areas mapped by a government agency or utility service providers.

HCD anticipates that projects that qualify under the Low- and Moderate-Income (LMI) National Objective are primarily using the Low- and Moderate-Income Area Benefit (LMA) category. Once a project's service area is identified, HUD Low- and Moderate-Income Summary Data is used to determine if least 51 percent of the residents are low- and moderate-income persons.

The Urgent Need National Objective requires that the project is designed to meet community development needs having a particular urgency. In the absence of substantial evidence to the contrary, a project is considered to address this National Objective if the design of the project is certified to alleviate existing conditions which pose a serious and immediate threat to the health or welfare of the community which are of recent origin or which recently became urgent, that the Subrecipient is unable to finance the activity on its own, and that other sources of funding are not available. A condition is generally considered to be of recent origin if it developed or became critical within 18 months preceding the certification by the Subrecipient.

### c. Eligible Applicants

The eligible applicants for Infrastructure Recovery Program funds are municipal and county governments that received FEMA Public Assistance funds for permanent infrastructure projects (Categories C through G) related to the DR-4382 or DR-4407 disaster events and/or are listed as a subrecipient jurisdiction under the 2018 CDBG Disaster Recovery Multifamily Housing Program.

## Eligible Applicants

- Butte County
- Lake County
- Los Angeles County
- Shasta County
- Ventura County
- City of Agoura Hills
- City of Anderson
- City of Calabasas
- City of Chico
- City of Gridley
- City of Lakeport
- City of Los Angeles
- City of Malibu
- City of Oroville
- City of Redding
- City of Shasta Lake
- City of Thousand Oaks
- City of Westlake Village
- Town of Paradise

Eligible Applicants may submit projects located anywhere within the unit of local government’s jurisdiction in accordance with the eligibility requirements outlined in Sections 2.4, 2.5, 2.6, and 2.7 below. As needed, Eligible Applicants may collaborate with other eligible entities, units of local government, or special districts in the implementation of the DR-Infrastructure Program.

Subrecipients may submit applications on behalf of another jurisdiction or special district. If the application is approved by HCD, the Subrecipient may enter into an agreement with another jurisdiction or special district for implementing the project. Prior to HCD issuing a Notice to Proceed (NTP) for the project, the Subrecipient must submit a monitoring plan for the unit of local government or special district working under the agreement. The monitoring plan shall demonstrate that the Subrecipient has the capacity to oversee work of the local government or special district.

Agreements with other units of local government, special districts, and other eligible entities are limited to program implementation support and the Subrecipient cannot allocate project funding directly to other government entities unless the Subrecipient, during the Due Diligence process, notifies HCD of its intent to make a unit of local government or special district a subrecipient for a potential project. In such instances, the following two (2) conditions must be special conditions in the MSA that the Subrecipient needs to clear prior to HCD issuing a Notice to Proceed for the specific potential project(s):

- 1.) HCD approves the Subrecipient’s monitoring plan of the unit of local government or special district for the proposed project. The monitoring plan must demonstrate that the Subrecipient has sufficient understanding of the program requirements, the applicable federal and state regulations, and the capacity to monitor the work of its subrecipients.
- 2.) Subrecipient agreements must be approved by HCD. If a Subrecipient is a CDBG entitlement community and has a subrecipient agreement in place, they may use the agreement as a template or an amended agreement, if approved by HCD.

If the preceding two (2) conditions are met, the Subrecipient can make the units of local government or special districts subrecipients and can directly allocate funds from the Subrecipient’s DR-Infrastructure allocation for individual projects approved by HCD during the Project Application process and for which HCD issues a Notice to Proceed. For all cases in accordance with 83 FR 5844 Section 51(k), HCD must also assess the Subrecipient’s capacity to execute and monitor the proposed project(s) as a factor in the project review. Additionally, for CDBG-DR funds to be used as the non-federal cost share local match, the project must be a HUD eligible activity and must meet a HUD national objective, per CDBG-DR rules and regulations. Similarly, the project must fully comply with all applicable federal rules and regulations, to include Davis Bacon and related acts, Section 3, Section 504, procurement, environmental review and all other CDBG-DR, cross-cutting, state and local applicable statutes, rules and regulations.

Special note, Paradise Irrigation District is an eligible applicant under the Town of Paradise and will be subject to the subrecipient requirements

## 6. CDBG-DR Infrastructure Program Policies and Procedures

Full CDBG-DR Infrastructure Program Policies and Procedures can be found here:

<https://www.hcd.ca.gov/community-development/disaster-recovery-programs/cdbg-dr/cdbg-dr-2018/docs/18DRINF18MITRIPPoliciesandProceduresv2.pdf>

## 7. Town of Paradise Notices of Intent Round 1 & Round 2

Through the development of the Town’s Community Long Term Recovery Plan and Transportation Master Plan, the Town of Paradise prepared a list of eligible projects to be considered in the CDBG-DR Infrastructure Notice of Intent phase for both Round 1 (LMI-eligible projects) and Round 2 (all other non-LMI projects). Action taken by Paradise Town Council on February 8, 2022 included the adoption of Resolution No. 2022-12, A Resolution of the Town Council of the Town of Paradise Declaring Certain Projects Critical to Camp Fire Recovery with Unfunded Needs for Consideration in the CDBG-DR Infrastructure Program. A copy of the project list adopted by Council and submitted for consideration is provided below:

Project Title	National Objective	Overall Priority	LMI Priority	Urgent Need Priority	Long Term Recovery Plan Tier and Project Category	Total Cost	Total Unmet Need
Paradise Sewer Project	Urgent Need	1	N/A	1	1 - Sewer System	\$184,675,000	\$182,275,000
On-System Road Rehabilitation	Urgent Need-Match	2	N/A	2	1 - Evacuation Routes	\$55,439,200	\$13,859,800
Off-System Road Rehabilitation	Urgent Need-Match	3	N/A	3	1 - Evacuation Routes	\$45,043,700	\$11,260,925
Roe Road Extension Phase 1 (S Libby to Pentz)	LMI	4	1	N/A	1 - Missing Road Segment	\$60,400,000	\$60,400,000
Roe Road Phase 2 (SR-191 to S Libby)	LMI	5	2	N/A	1 - Missing Road Segment	\$42,622,089	\$42,622,089
Roe Road Phase 3 (Scottwood to SR-191)	LMI	6	3	N/A	1 - Missing Road Segment	\$11,105,512	\$11,105,512
Pentz Road Widening (Town Limits to Skyway)	Urgent Need	7	N/A	4	1 - Evacuation Routes	\$88,865,680	\$88,865,680
Early Warning System	Urgent Need-Match	8	N/A	5	1 - Emergency Notification	\$3,207,400	\$3,200,000
Elliott Road Extension (End to Pentz Road)	Urgent Need	9	N/A	6	1 - Missing Road Segment	\$7,050,742	\$7,050,742
Neal Road Rehabilitation	Urgent Need-Match	10	N/A	7	1 - Evacuation Routes	\$1,588,300	\$397,075
Off-System Culvert Repair	Urgent Need-Match	11	N/A	8	1 - Evacuation Routes	\$750,000	\$187,500
Buschmann Extension between Clark & Libby	LMI	12	4	N/A	1 - Missing Road Segment	\$24,043,221	\$24,043,221
Sawmill Extension south to Roe Road Extension	LMI	13	5	N/A	1 - Long Dead-End Streets	\$3,337,373	\$3,337,373
Roe Road Phase 4 (Skyway to Neal)	Urgent Need	14	N/A	9	1 - Missing Road Segment	\$20,380,141	\$20,380,141



Roe Road Phase 5 (Neal to Scottwood)	Urgent Need	15	N/A	10	1 - Evacuation Routes	\$21,049,870	\$21,049,870
Upper Skyway Widening (Bille to Pentz)	Urgent Need	16	N/A	11	1 - Evacuation Routes	\$49,067,998	\$49,067,998
Neal Road Widening (Town Limits to Skyway)	Urgent Need	17	N/A	12	1 - Evacuation Routes	\$25,308,880	\$25,308,880
On-System Culvert Replacement	Urgent Need-Match	18	N/A	13	1 - Evacuation Routes	\$811,000	\$202,750
Upper Clark Widening (Wagstaff to Skyway)	Urgent Need	19	N/A	14	1 - Evacuation Routes	\$25,007,475	\$25,007,475
Elliott Road Extension (west of Skyway)	Urgent Need	20	N/A	15	1 - Missing Road Segment	\$850,011	\$850,011
Category 4 Tree Removal	Urgent Need-Match	21	N/A	16	1 - Standing Burnt Tree Removal	\$11,747,100	\$2,936,782
Category 4 Tree Removal Additional Assistance	Urgent Need-Match	22	N/A	17	1 - Standing Burnt Tree Removal	\$10,000,000	\$2,500,000
General Plan Update	Urgent Need	23	N/A	18	1 - Planning and Zoning	\$1,200,000	\$1,200,000
Sewer Service Area Specific Plan	Urgent Need	24	N/A	19	1 - Sewer System	\$800,000	\$800,000
Defensible Space Code Enforcement	Urgent Need-Match	25	N/A	20	1 - Updated codes/standards	\$691,460	\$172,865
On-System Hardscape Replacement	Urgent Need-Match	26	N/A	21	1 - Walkable Downtown	\$767,100	\$191,775
Storm Drain Master Plan (HMGP)	Urgent Need-Match	27	N/A	22	2 - Sustainable Stormwater	\$513,774	\$128,444
Residential Ignition Resistant Program	Urgent Need-Match	28	N/A	23	2 - Education Program	\$8,465,986	\$2,116,497
ATP 1 - Skyway Connectivity Project	Urgent Need	29	N/A	24	1 - Interconnect Path System	\$4,461,689	\$4,461,689
ATP 3 - Pentz Pathway Project Phase II	Urgent Need	30	N/A	25	1 - Interconnect Path System	\$27,164,964	\$26,464,964
ATP 2 - Paradise ATP Gateway Project	Urgent Need	31	N/A	26	1 - Interconnect Path System	\$10,066,526	\$9,516,526
ATP 4 - Oliver Curve Pathway Project Phase I	Urgent Need	32	N/A	27	1 - Interconnect Path System	\$4,906,900	\$5,073,547
Foster/Black Olive Intersection Improvements	Urgent Need	33	N/A	28	1 - Evacuation Routes	\$5,000,000	\$5,000,000
Buschmann Extension West of Foster Road	Urgent Need	34	N/A	29	1 - Missing Road Segment	\$3,290,571	\$3,290,571
Intersection Improvements Skyway/Pentz	Urgent Need	35	N/A	30	1 - Evacuation Routes	\$4,500,000	\$4,500,000
Moore Road (Public, pave)	Urgent Need	36	N/A	31	1 - Missing Road Segment	\$1,436,639	\$1,436,639
Pentz/Pearson Intersection Improvements	Urgent Need	37	N/A	32	1 - Evacuation Routes	\$4,500,000	\$4,500,000
Clark Class I (Skyway to Pearson)	Urgent Need	38	N/A	33	1 - Interconnect Path System	\$21,554,691	\$21,554,691
Pearson Class I (Clark to Pentz)	Urgent Need	39	N/A	34	1 - Interconnect Path System	\$13,554,857	\$13,554,857
Elliott Class I (Skyway to Sawmill)	Urgent Need	40	N/A	35	1 - Interconnect Path System	\$12,718,561	\$12,718,561
Wagstaff Class I (North side Oliver to Clark)	Urgent Need	41	N/A	36	1 - Interconnect Path System	\$12,638,530	\$12,638,530
Libby Road Extension between Pearson & Elliott	Urgent Need	42	N/A	37	1 - Missing Road Segment	\$5,898,678	\$5,898,678
Changeable Message Sign Network	Urgent Need	43	N/A	38	1 - Emergency Notification	\$130,000	\$130,000
Forest Service Road	Urgent Need	44	N/A	39	1 - Missing Road Segment	\$1,706,351	\$1,706,351
Toyon Lane Improvements (Foster to Roe)	Urgent Need	45	N/A	40	1 - Missing Road Segment	\$3,880,420	\$3,880,420

On-System Sign Replacement	Urgent Need-Match	46	N/A	41	1 - Evacuation Routes	\$285,200	\$71,300
Traffic Management Center	Urgent Need	47	N/A	42	1 - Evacuation Routes	\$3,500,000	\$3,500,000
Traffic Signal Interconnect (Town of Paradise)	Urgent Need	48	N/A	43	1 - Evacuation Routes	\$10,500,000	\$10,500,000
Honey Run Road Improvements	Urgent Need	49	N/A	44	1 - Evacuation Routes	\$80,000	\$80,000
Reseeding Program	Urgent Need-Match	50	N/A	45	1 - Fuels Management Plan	\$53,600	\$13,400
Hazardous Fuel Reduction Program	Urgent Need-Match	51	N/A	46	1 - Fuels Management Plan	\$8,465,986	\$2,116,497
Bille Road Class I (Bille Park to Pentz Road)	Urgent Need	52	N/A	47	1 - Interconnect Path System	\$17,865,526	\$17,865,526
Sawmill Class I (Bille to Pearson)	Urgent Need	53	N/A	48	1 - Interconnect Path System	\$9,129,765	\$9,129,765
Valley View Class I (Valley Ridge Dr West to Oliver)	Urgent Need	54	N/A	49	1 - Interconnect Path System	\$8,973,865	\$8,973,865
Oliver Curve Pathway Project Phase II (Bille to Wagstaff)	Urgent Need	55	N/A	50	1 - Interconnect Path System	\$3,787,009	\$3,787,009
Skyway Class I (YKHT to Oliver Road)	Urgent Need	56	N/A	51	1 - Interconnect Path System	\$3,371,677	\$3,371,677
Pearson Roadway Reconfiguration (Skyway to Almond)	Urgent Need	57	N/A	52	1 - Evacuation Routes	\$600,000	\$600,000
Shay Lane Extension (to Center Pine Drive)	Urgent Need	58	N/A	53	1 - Long Dead-End Streets	\$849,633	\$849,633
Bille Road Extension east of Pentz	Urgent Need	59	N/A	54	1 - Missing Road Segment	\$2,598,810	\$2,598,810
Clark (spot widening b/t Wagstaff & Bille)	Urgent Need	60	N/A	55	1 - Evacuation Routes	\$1,684,300	\$1,684,300

A map of most infrastructure projects planned as a result of the development of the Town's Transportation Master Plan is found below.



Figure 1 - Paradise TMP Evacuation Projects Map

## 8. HCD Funding Allocation Announcement

On August 19, 2022, Governor Gavin Newsom announced federal grants totaling more than \$317 million to assist seven jurisdictions that continue to recover and rebuild after the 2018 federally declared disasters, when wildfires burned more than 1.6 million acres and destroyed hundreds of homes and businesses, particularly in the Town of Paradise, in Butte County.

The Community Development Block Grant-Disaster Recovery (CDBG-DR) funds announced are for foundational infrastructure projects that must be completed as communities work to build and rebuild needed housing, especially housing for low- and moderate-income Californians, and ensure that every household that was displaced in these disasters has the opportunity to return home.

A press release issued by the Governor’s office stated:

*Communities were awarded funding based on their unmet infrastructure needs and have the flexibility to use the grants to support projects based on their priorities to rebuild in a safe, sustainable, and resilient way. The Town of Paradise has been awarded nearly \$200 million, which will be used to construct critical projects that will accelerate their capacity to rebuild higher density and more affordable housing, business corridors, and critical evacuation routes in the event they face future disasters.*

The following is a full list of awards announced across the entire CDBG-DR Infrastructure Program:

<b><u>Local Jurisdiction</u></b>	<b><u>Total Awarded</u></b>
Butte County	\$72,722,679.61
City of Chico	\$12,388,409.65
City of Malibu	\$47,276.93
City of Redding	\$22,563,043.51
City of Shasta Lake	\$6,326,184.69
Los Angeles County	\$3,788,157.86
<b><u>Town of Paradise</u></b>	<b><u>\$199,592,735.75</u></b>
Total	\$317,428,488.00

## 9. Town of Paradise Allocation

The Town of Paradise was allocated \$199,592,735.75. This determination was made after a review of the Town’s eligible projects in respect to other jurisdictions eligible projects and available funding. As a result, the Town of Paradise was awarded 27% its overall eligible unmet need. The Town’s allocation is also inclusive of Paradise Irrigation District’s four eligible projects. The Town of Paradise plans to administer CDBG-DR Program Guidelines for subrecipients and follow the HCD formulate to distribute 27% of Paradise Irrigation District’s eligible projects. Paradise Irrigation District submitted \$13,030,000 in eligible CDBG-DR Infrastructure projects. Apportioned at 27%, \$3,518,100 would be the Town’s subrecipient allocation to Paradise Irrigation District, reducing the Town’s available funding to \$196,074,635.75.

Separate from the CDBG-DR Infrastructure total of \$317M available, Action Plan Amendment 2 was issued by HCD which identified an additional \$30M to be allocated to the Town of Paradise for the Paradise Sewer Project design phase.

Combined, the Town is tasked with administering a total \$229,592,735.75 (inclusive of the Paradise Sewer Project Design phase funding), in a long-term partnership with California Housing & Community Development to create generational change in a stronger, more resilient Paradise.

## 10. Town of Paradise Project Grant List

Critically, the Town will define the budget allocations to implement specific projects which meet the objectives and priorities of the CDBG-DR program and community by:

- Stabilizing the Town's Fiscal Sustainability Plan through the allocation of eligible Disaster Recovery and Mitigation projects with previously unfunded match requirements.
- Implement a diverse array of infrastructure projects identified through the extensive planning of the 2019 Community Long-Term Recovery Plan and 2020 Transportation Master Plan. Projects identified will either: increase evacuation capacity, connect dead-end roads or complete missing road segments which would have been utilized during the 2018 Camp Fire evacuation. Once constructed these infrastructure projects will be available for use in future evacuation scenarios in the Town.

The Long Term Recovery Plan and Transportation Master Plan priorities have been incorporated into a CDBG-DR Project Grant List which fully utilizes the Town's allocation. The Project Priorities, Titles, Types, National Objectives, Total Costs and CDBG-DR Grant Amounts are provided on the following page:

**CDBG-DR Project Grant List 24-0109**

Priority	Project Title	Project Type	National Objective	Total Project Cost	CDBG-DR Project Grant Amount
1	Paradise Sewer Project (Design APA-2)	New Infrastructure Project	UN	\$39,791,100	\$30,000,000
1	Paradise Sewer Project (Construction)	New Infrastructure Project	UN	\$253,000,000	\$15,000,000
2	Roe Road Phase 1	New Infrastructure Project	LMI	\$66,000,000	\$64,200,000
2	Roe Road Phase 2	New Infrastructure Project	LMI	\$66,000,000	\$4,200,000
3	Pentz Road Widening	New Infrastructure Project	UN	\$51,000,000	\$51,000,000
3	Skyway/Pentz Intersection Improvements	New Infrastructure Project	UN	\$4,500,000	\$4,500,000
3	Pentz Pathway Project Phase II	New Infrastructure Project	UN	\$23,293,000	\$1,000,000
4	Upper Skyway Widening (Bille to Wagstaff)	New Infrastructure Project	UN	\$15,000,000	\$15,000,000
4	Skyway Connectivity Project (Bille to Wagstaff)	New Infrastructure Project	UN	\$6,810,000	\$250,000
5	Paradise ATP Gateway Project (Neal Road Class I)	New Infrastructure Project	UN	\$13,068,000	\$800,000
6	Elliott/Nunneley Road Extension	New Infrastructure Project	UN	\$7,500,000	\$7,500,000
7	Oliver Curve Pathway Phase I	New Infrastructure Project	UN	\$13,100,000	\$3,100,000
8	Foster/Black Olive Intersection Improvements	New Infrastructure Project	UN	\$3,500,000	\$3,500,000
9	Forest Service Road Improvements	New Infrastructure Project	UN	\$3,000,000	\$3,000,000
M1	On-System Road Rehabilitation	Match - FHWA ER	UN/LMI*	\$55,439,200	\$13,800,000
M2	Off-System Road Rehabilitation	Match - FEMA PA	UN/LMI*	\$45,043,700	\$7,308,635
M3	Early Warning System	Match - FEMA HMGP	UN/LMI*	\$3,207,400	\$756,000
M4	Hazardous Fuel Reduction Program	Match - FEMA HMGP	UN	\$8,465,986	\$475,000
M5	Neal Road Rehabilitation	Match - FHWA ER	UN	\$1,588,300	\$425,000
M6	On-System Culvert Replacement	Match - FHWA ER	UN	\$811,000	\$100,000
M7	On-System Hardscape Replacement	Match - FHWA ER	UN	\$767,100	\$75,000
M8	Off-System Culvert Repair	Match - FEMA PA	UN	\$750,000	\$50,000
M9	On-System Sign Replacement	Match - FHWA ER	UN	\$285,200	\$35,000
			<b>TOTAL</b>	<b>\$685,019,986</b>	<b>\$226,074,635</b>

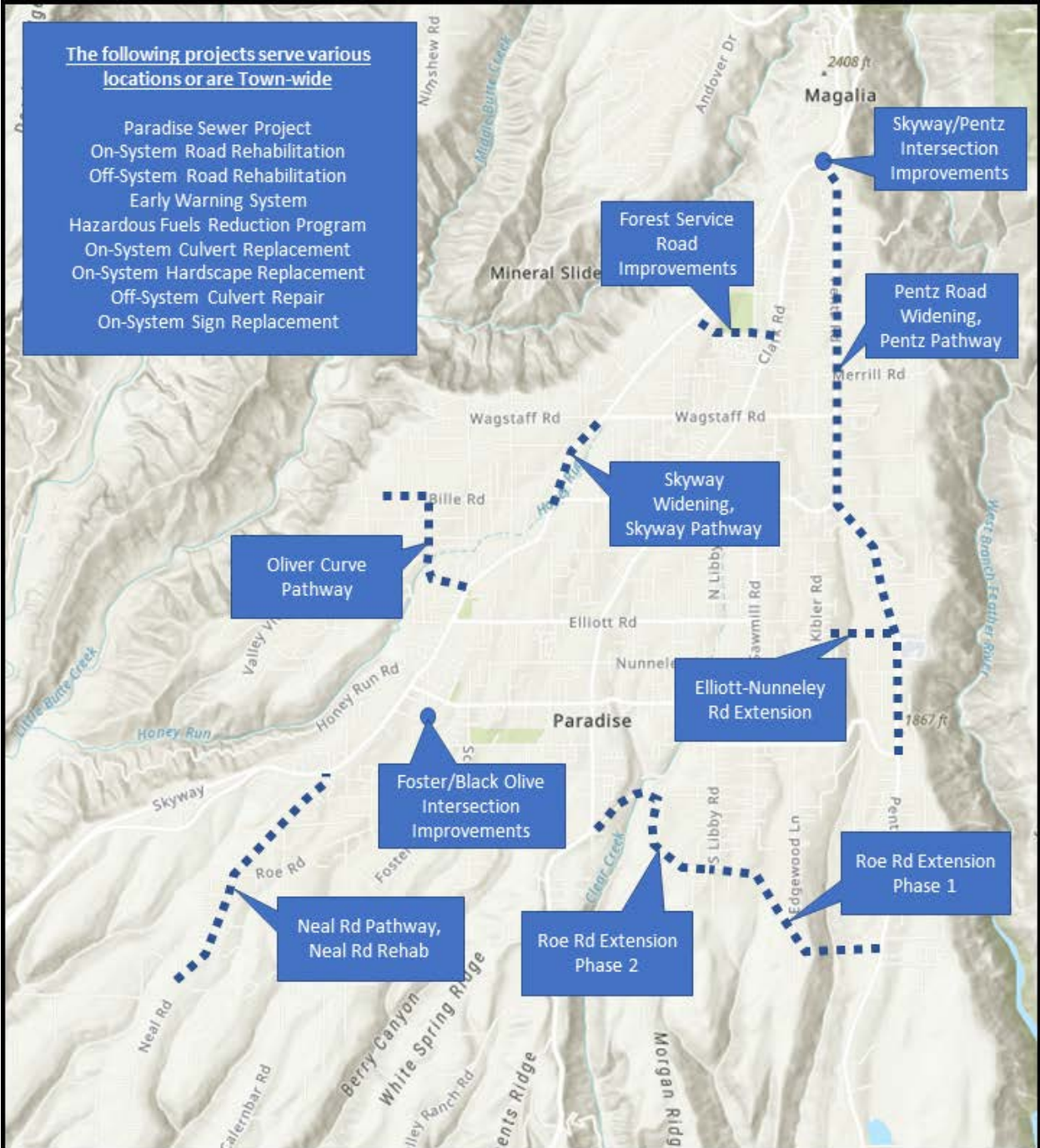
Key components and statistics of the proposed use of the overall allocation are specified below:

- **Previously Unfunded Match Requirements:** \$23,024,635 in funding dedicated to fulfilling existing match requirements, including road rehabilitation and early warning system.
- **New Standalone Infrastructure Projects:** \$203,050,000 in funding dedicated to constructing new, standalone infrastructure projects.
  
- **National Objective, Low Moderate Income:** \$68,400,000 in funding qualifies towards Low Moderate Income eligible projects.
- **National Objective, Urgent Need:** \$132,710,000 in funding qualifies towards Urgent Need eligible projects.
- **National Objective, Urgent Need/Low Moderate Income\*:** \$21,864,635 in funding is dedicated to projects which have town-wide service areas, including areas which qualify as Low Moderate Income.
  
- **Paradise Long Term Recovery Plan Project Statistics:** Below is a breakdown of the overall allocation's use divided by the Town's Long Term Recovery Plan Categories
  - o Evacuation Routes: \$95,718,635
  - o Sewer System: \$45,000,000
  - o Missing Road Segments: \$78,900,000
  - o Interconnected Path System: \$5,150,000
  - o Emergency Notification System: \$756,000
  - o Fuels Management: \$475,000
  - o Walkable Downtown: \$75,000
  
- **Paradise Sewer Project:** \$45,000,000 in dedicated funding. HCD allocation of \$30,000,000 for the design phase of the project and an additional \$15,000,000 is recommended from the overall Town allocation to be used towards the construction of the project.
  
- **Overall Allocation Leverage:** The Town of Paradise's total \$226,074,635 is proposed to leverage a total of \$685,019,986 combined from at least six separate funding sources. Note, of the total leveraged amount, the Town of Paradise is still seeking \$253M for the construction of the Paradise Sewer Project as well as \$28M for Roe Road Extension Phase 2 Project.

An overall Project Location Map can be found on the next page.

Project fact sheets which cover location, scope and submitted costs may be found in Appendix A: Recommended Projects.

The recommended projects represent the highest and best use of the CDBG-DR investment while establishing a critical network of projects, when taken globally, will transform Paradise for the next 100 years and more.



# CDBG-DR Project Grant List Project Location Map





## 11. Town of Paradise Reserve Project List

The original CDBG-DR Action Plan set a reserve project list. Action Plan Amendment 1 established the following policy: Any newly available funding (ie selected projects come in under budget or additional external funding is secured) within the Town’s CDBG-DR allocation will be dedicated to the Paradise Sewer Project construction phase. This policy allows the program to keep pace with long-range planning and development of critical infrastructure projects.

## 12. Future Projects

The Town’s overall project unmet need identified for the purposes of CDBG-DR Infrastructure consideration exceeded \$800M – while the \$199M infrastructure investment will clearly signify an unprecedented investment in the community’s safety and housing recovery, much remains unfunded. Projects not selected will remain documented as critical priorities within the Town’s overall recovery vision for other funding considerations at the local, state and federal levels.

## 13. Project Schedule Forecast

For the Recommended Project List, staff created a potential schedule of implementation and deliver, forecasting work to continue through 2029 on major efforts, with some projects minor projects reaching closeout in 2024. The Project Schedule Forecast is shown on the next page.

**CDBG-DR Project Grant List 24-0109 Schedule**

Priority	Project Title	Total Project Cost	CDBG-DR Project Grant Amount	Estimated Environmental Review Completion	Estimated Design & ROW Completion	Estimated Construction Completion
1	Paradise Sewer Project (Design APA-2)	\$39,791,100	\$30,000,000	2024	2025	N/A
1	Paradise Sewer Project (Construction)	\$253,000,000	\$15,000,000	2024	2025	2027
2	Roe Road Phase 1	\$66,000,000	\$64,200,000	2025	2026	2028
2	Roe Road Phase 2	\$66,000,000	\$4,200,000	2025	2026	2028
3	Pentz Road Widening	\$51,000,000	\$51,000,000	2024	2026	2027
3	Skyway/Pentz Intersection Improvements	\$4,500,000	\$4,500,000	2024	2025	2026
3	Pentz Pathway Project Phase II	\$23,293,000	\$1,000,000	2024	2026	2027
4	Upper Skyway Widening (Bille to Wagstaff)	\$15,000,000	\$15,000,000	N/A	N/A	2027
4	Skyway Connectivity Project (Bille to Wagstaff)	\$6,810,000	\$250,000	2025	2025	2026
5	Paradise ATP Gateway Project (Neal Road Class I)	\$13,068,000	\$800,000	2024	2025	2026
6	Elliott/Nunnelely Road Extension	\$7,500,000	\$7,500,000	2025	2025	2026
7	Oliver Curve Pathway Phase I	\$13,100,000	\$3,100,000	2024	2025	2027
8	Foster/Black Olive Intersection Improvements	\$3,500,000	\$3,500,000	2025	2025	2026
9	Forest Service Road Improvements	\$3,000,000	\$3,000,000	2025	2025	2026
M1	On-System Road Rehabilitation	\$55,439,200	\$13,800,000	Complete	Complete	2027
M2	Off-System Road Rehabilitation	\$45,043,700	\$7,308,635	Complete	Complete	2026
M3	Early Warning System	\$3,207,400	\$756,000	Complete	Complete	2024
M4	Hazardous Fuel Reduction Program	\$8,465,986	\$475,000	2024	2024	2027
M5	Neal Road Rehabilitation	\$1,588,300	\$425,000	Complete	Complete	2026
M6	On-System Culvert Replacement	\$811,000	\$100,000	Complete	Complete	Complete
M7	On-System Hardscape Replacement	\$767,100	\$75,000	Complete	Complete	Complete
M8	Off-System Culvert Repair	\$750,000	\$50,000	Complete	Complete	Complete
M9	On-System Sign Replacement	\$285,200	\$35,000	Complete	Complete	Complete
	<b>Total</b>		<b>\$226,074,635</b>			

## 14. Issues to Watch

### a. Low Moderate Income Projects

At this time, the Town has an additional requirement to implement \$73,984,047.67 of its allocation towards Low Moderate Income eligible requirements. As submitted, the Town's project list relied on pre-fire income data which, in turn, made a handful of projects LMI eligible. Considering Amendment 2, the Town is nearly meeting this requirement with \$68,400,000 in funding is proposed towards pre-fire LMI qualifying projects.

### b. HUD Environmental Review Waiver for Completed FHWA Emergency Relief Projects

Town and HCD staff are actively coordinating with HUD relating to the use of CDBG-DR Infrastructure funding to fill match requirements on four completed Federal Highways Administration (FHWA) Emergency Relief Program Projects. These FHWA projects underwent their own independent environmental review processes, however, due to the fact that the projects were complete or partially complete in advance of a separate HUD-compliant environmental review process, use of CDBG-DR funding is contested. Staff anticipates updates from HUD/HCD in 2024 which would provide guidance to proceed with reimbursement requests.

### c. Program Schedule

With ongoing development of the CDBG-DR Infrastructure program, administrators with HCD have established an expenditure deadline of December 2025. At this time, of the Town's project list proposed, only a handful could feasibly be completed in this amount of time. Staff will be working continuously with HCD and HUD to establish a reasonable program end date which allows for the projects to be completed as efficiently as possible.

## 15. Next Steps

In late August 2022, staff received notice of a potential schedule for next steps, summarized on the next page:

**Step 1) Allocation Notices Released (Complete)**

**Step 2) Due Diligence (Complete)**

**Step 3) Authorizing Resolution (Complete)**

**Step 4) Master Standard Agreement (MSA) Release (Complete)**

**Step 5) Project Application (underway)**

Once the MSAs are executed, HCD will release project applications in Grants Network.

Subrecipients must complete an application for each project and submit it to HCD via Grants Network. Only projects submitted under the Notice of Intent (NOI) can be submitted as project applications.

Projects originally submitted under the NOI as Urgent Need can be submitted as an LMI project (if the LMI percentage or service area changed), but a project submitted under the NOI as LMI cannot be submitted as Urgent Need.

**Step 6) Notice to Proceed (NTP)**

An NTP is the final step before the subrecipient can begin expending funds, other than funds that were pre-approved by HCD for pre-construction work. An NTP requires the subrecipient to provide an updated schedule and performance milestones for each project prior to commencing work.

As described in the MSA, once an application is approved, each project must receive an NTP. An NTP will not be issued until Steps 1-5 are complete.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	1
<b>Project Name</b>	Paradise Sewer Project (Design)
<b>Project Type</b>	New Infrastructure Project
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Sewer System
<b>Project Total Cost</b>	\$39,791,100
<b>CDBG-DR Project Grant Amount</b>	\$30,000,000
<b>Environmental Completion Year</b>	2023
<b>Design &amp; ROW Completion Year</b>	2024
<b>Construction Completion Year</b>	N/A

### **Project Scope**

The Paradise Sewer Project includes all of the components and activities necessary to convert the core part of the Town (the Sewer Service Area) from on-site septic tank treatment to wastewater conveyance and treatment at the Chico Water Pollution Control Plant (WPCP).

### **Project Description**

The next phase of the Paradise Sewer Project will be preparation of the engineering design of project components. During this design phase, several other key activities will occur to prepare the project for construction, namely right-of-way (ROW) acquisition, environmental permitting and connection payment. Each of these items are described in further detail below.

### **Engineering Design**

The project consists of two major components—the core collection system and the export pipeline system. Because these two components are very different (sit in different geographical locations and are made up of slightly different types of infrastructure), each component will be designed by a different engineering firm. These two design efforts will go on in parallel, and will take approximately 18 to 24 months to complete. Both designs will be typical civil engineering efforts involving the creation of design drawings and specifications.

The engineering design effort will also involve obtaining certain types of construction permits, such as permission to cross highways and railroads.



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### **ROW Acquisition**

Before a project can be constructed, the owner must acquire the legal right to install the infrastructure. For the core collection system, a vast majority of the sewer pipes and small pump stations will be installed within Town of Paradise right-of-way (roads). However, there may be several small areas where ROW will need to be acquired from private property owners, such as short lengths of sewer that must cross private land.

For the export pipeline system, two ROW situations exist:

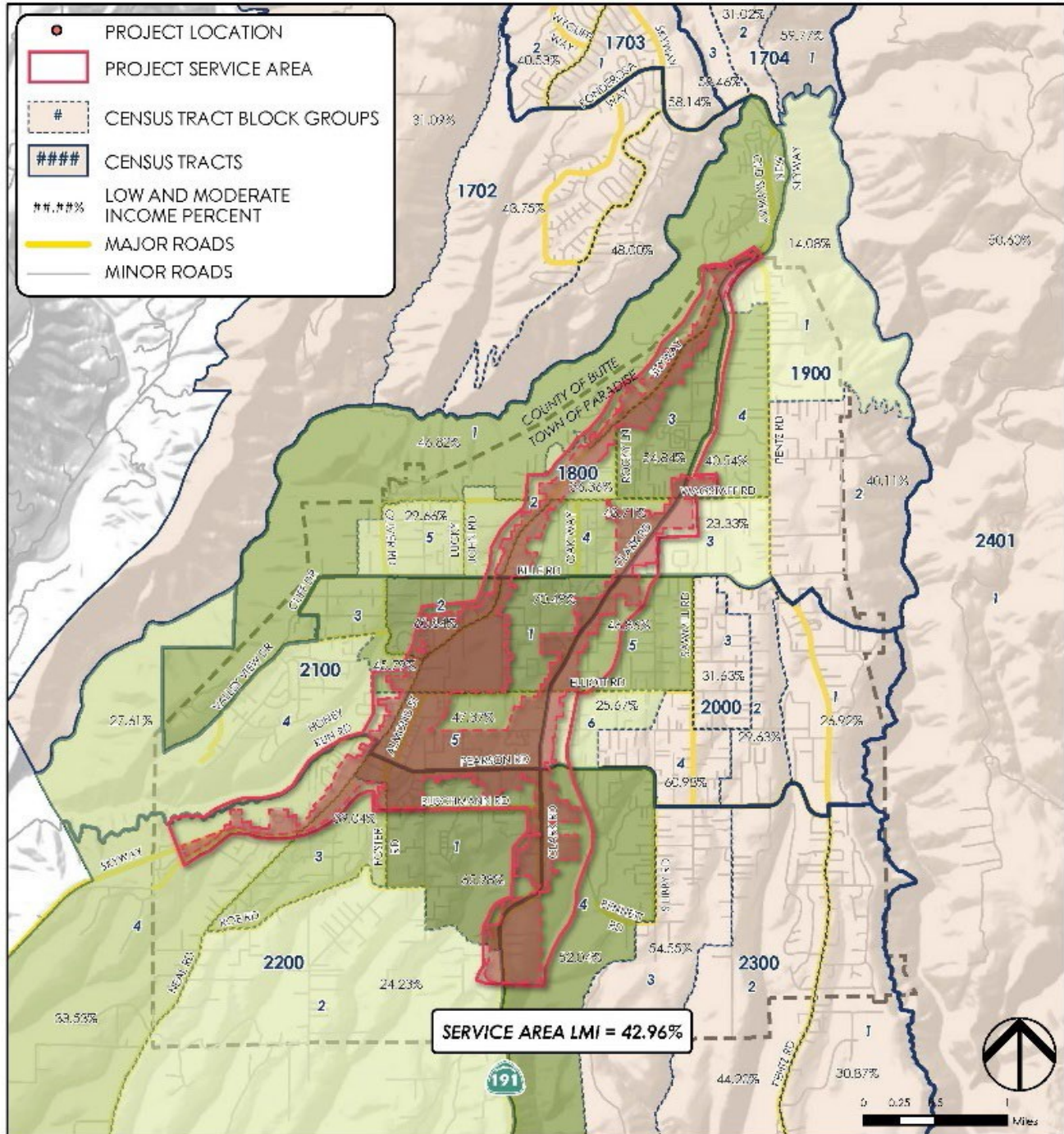
- Of the 18.2 miles of export pipeline length, 17.1 miles will be installed within Butte County road rights-of-way. The Town will obtain an encroachment permit from Butte County for this installation.
- For the 1.1 miles of export pipeline that crosses private property, the Town will obtain ROW from private property owners. This ROW will primarily be in the form of temporary (construction) and permanent easements for buried pipelines. The permanent easements usually allow for some continued surface use of the land over the pipeline, with certain limitations (e.g., no permanent structures placed over the top of the pipeline).

### **Environmental Permitting**

The Town will need to obtain environmental permits from certain state and federal agencies. Because obtaining these permits is often very complex and time consuming, they are usually handled as separate activities during the design phase. Because the core collection system will be installed within the Town of Paradise, in areas that have already been developed, it is likely that no significant environmental permits will be needed for that component. The export pipeline system crosses several water bodies (creeks) and 1.1 miles of undeveloped land, which will likely trigger the need to obtain environmental permits from the U.S. Army Corps of Engineers, U.S. Fish and Wildlife Service, National Marine Fisheries Service, California Department of Fish and Wildlife, and the Central Valley Regional Water Quality Control Board.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	1
<b>Project Name</b>	Paradise Sewer Project (Construction)
<b>Project Type</b>	New Infrastructure Project
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Sewer System
<b>Project Total Cost</b>	\$253,000,000
<b>CDBG-DR Project Grant Amount</b>	\$15,000,000
<b>Environmental Completion Year</b>	2024
<b>Design &amp; ROW Completion Year</b>	2025
<b>Construction Completion Year</b>	2026

**Project Scope**

The Paradise Sewer Project includes all of the components and activities necessary to convert the core part of the Town (the Sewer Service Area) from on-site septic tank treatment to wastewater conveyance and treatment at the Chico Water Pollution Control Plant (WPCP).

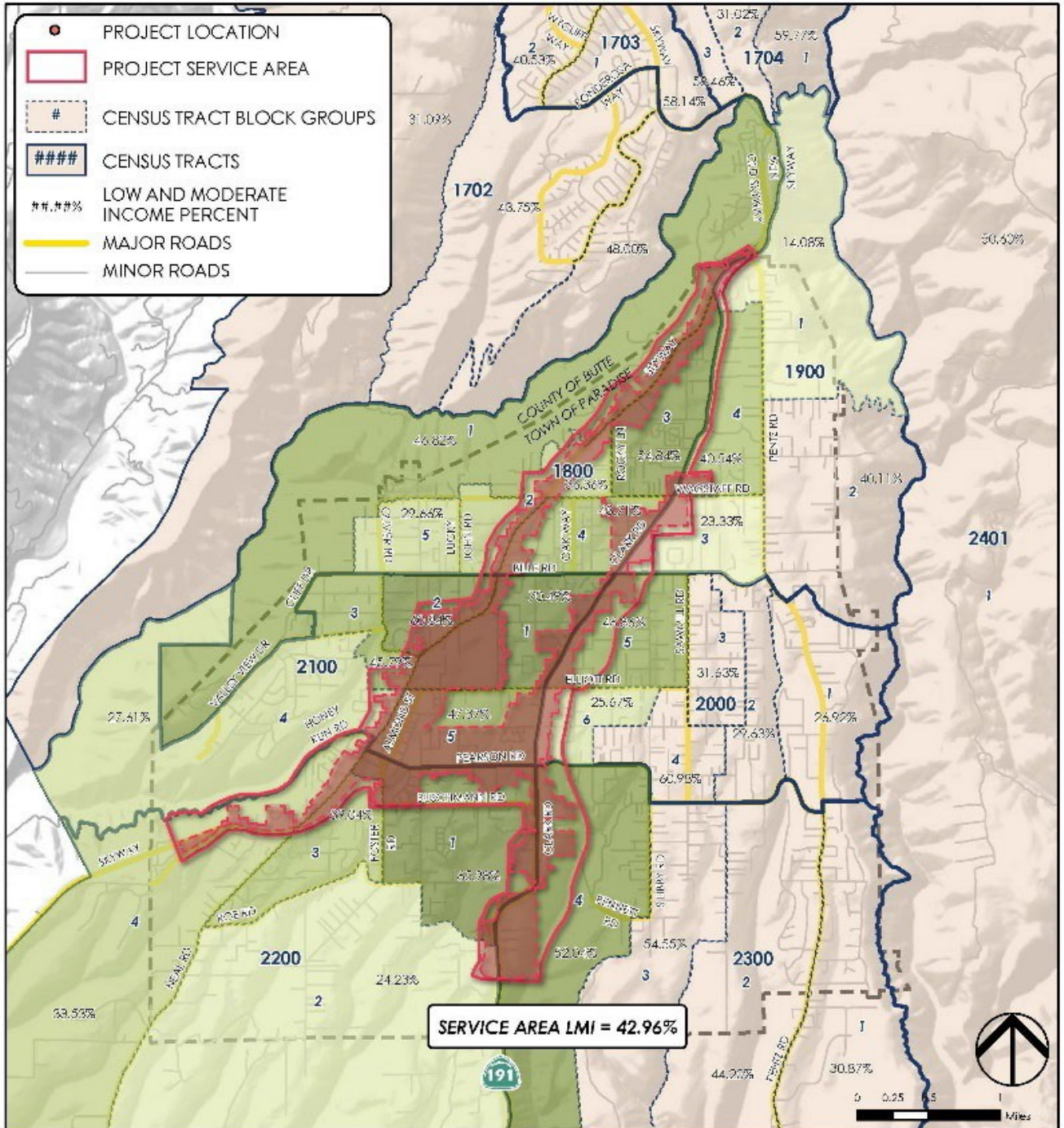
**Project Description**

The Paradise Sewer Project consists of three primary components: 1) a wastewater collection system consisting of service connections of individual parcels within a designated service area to sewer lines and lift stations; 2) an 18-mile export pipeline to convey Paradise wastewater to the Chico WPCP; and 3) connection to the Chico WPCP where the Paradise wastewater will be treated.





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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	2
<b>Project Name</b>	Roe Road Phase 1
<b>Project Type</b>	New Infrastructure Project
<b>HUD National Objective</b>	Low-Moderate Income
<b>Long-Term Recovery Plan</b>	Missing Road Segment
<b>Project Total Cost</b>	\$66,000,000
<b>CDBG-DR Project Grant Amount</b>	\$64,200,000
<b>Environmental Completion Year</b>	2025
<b>Design &amp; ROW Completion Year</b>	2026
<b>Construction Completion Year</b>	2028

**Project Scope**

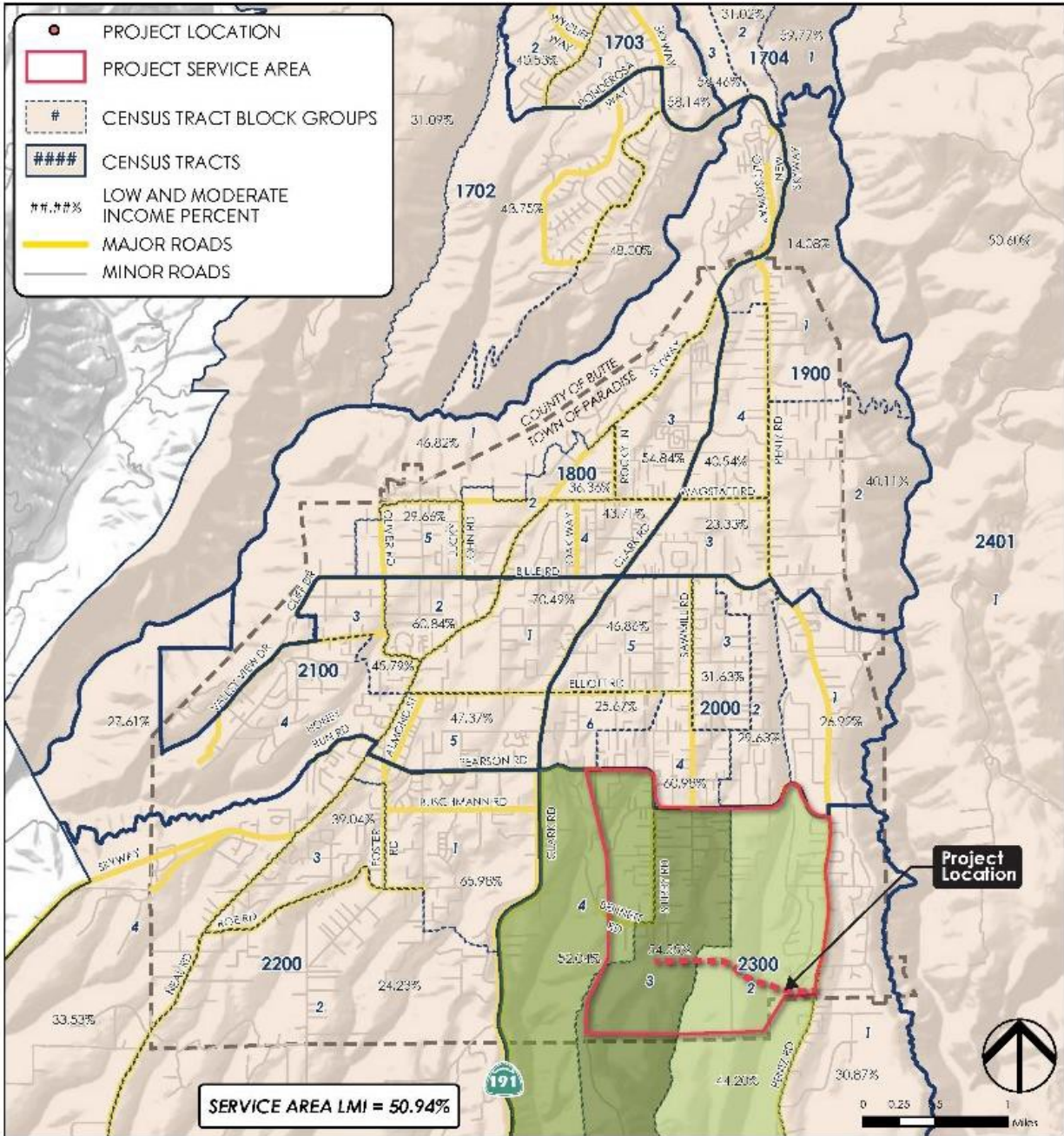
Roe Road Phase 1 Project will construct a new three lane 1.25-mile roadway with wide shoulders. The project will include a Class I multi-use pathway, ADA curb ramps, storm drain infrastructure, and intersections with Pentz Road, Edgewood Lane, and South Libby Road.

**Project Description**

Roe Road Phase 1 project is part of a five-phase project to construct a new east-west roadway connection between Skyway and Pentz Road. The proposed project will construct 1.25-miles of roadway between South Libby Road and Pentz Road to improve circulation and connectivity throughout the Town and provide alternative ingress/egress routes for residential neighborhoods. The project will also construct a parallel Class I bikeway for travel by people walking and bicycling and provides first responders a dedicated bi-directional travel lane during emergency events.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	2
<b>Project Name</b>	Roe Road Phase 2
<b>Project Type</b>	New Infrastructure Project
<b>HUD National Objective</b>	Low-Moderate Income
<b>Long-Term Recovery Plan</b>	Missing Road Segment
<b>Project Total Cost</b>	\$66,000,000
<b>CDBG-DR Project Grant Amount</b>	\$4,200,000
<b>Environmental Completion Year</b>	2025
<b>Design &amp; ROW Completion Year</b>	2026
<b>Construction Completion Year</b>	2028

**Project Scope**

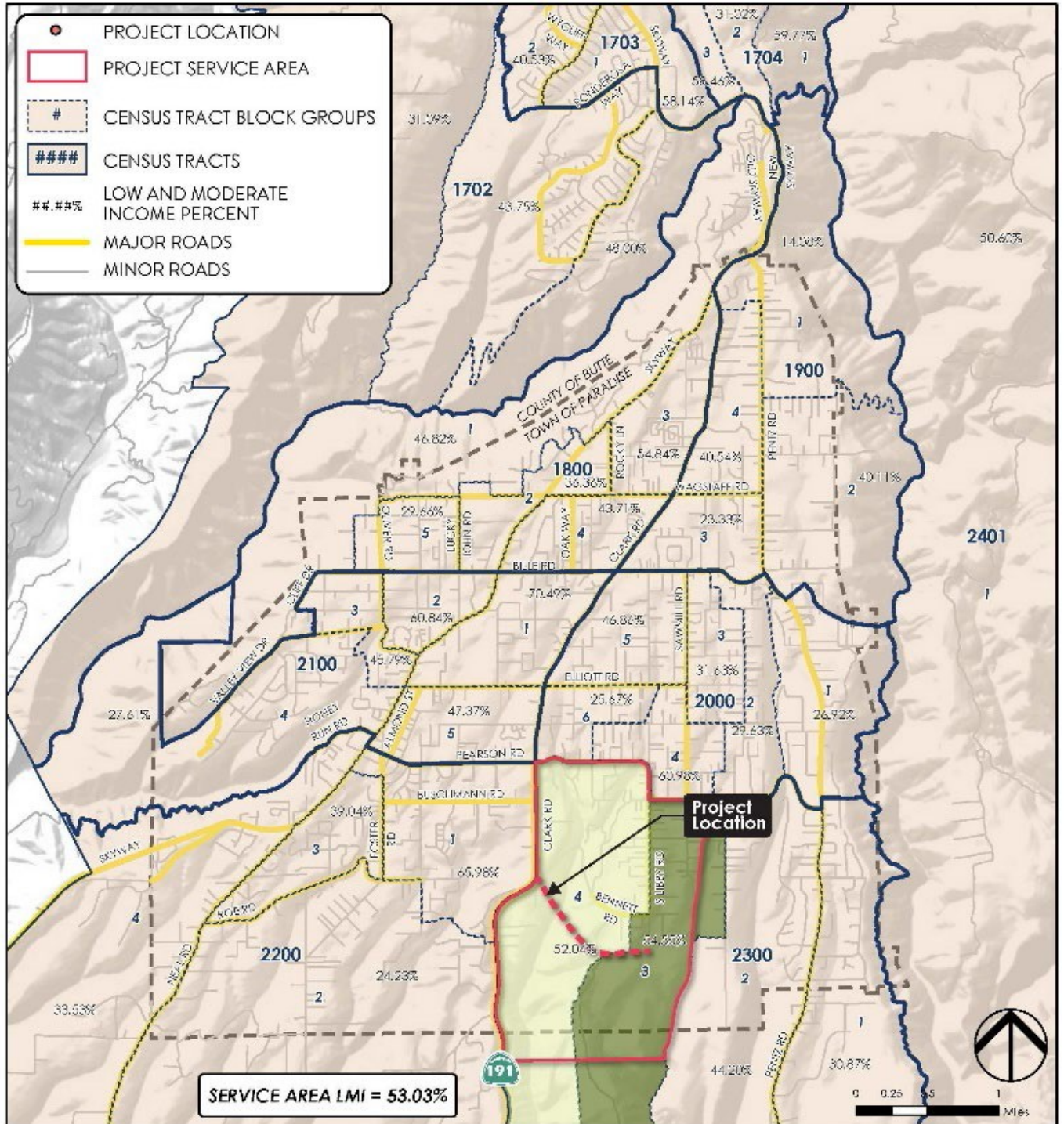
Roe Road Phase 2 Project will construct a new three lane 1.3-mile roadway with wide shoulders. The project will include a Class I multi-use pathway, ADA curb ramps, storm drain infrastructure, and intersections with South Libby Road and Clark Road.

**Project Description**

Roe Road Phase 2 project is part of a five-phase project to construct a new east-west roadway connection between Skyway and Pentz Road. The proposed project will construct 1.3-miles of roadway between South Libby Road and Clark Road to improve circulation and connectivity throughout the Town and provide alternative ingress/egress routes for residential neighborhoods. The project will also construct a parallel Class I bikeway for travel by people walking and bicycling and provides first responders a dedicated bi-directional travel lane during emergency events.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	3
<b>Project Name</b>	Pentz Road Widening
<b>Project Type</b>	New Infrastructure Project
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Evacuation Routes
<b>Project Total Cost</b>	\$51,000,000
<b>CDBG-DR Project Grant Amount</b>	\$51,000,000
<b>Environmental Completion Year</b>	2024
<b>Design &amp; ROW Completion Year</b>	2026
<b>Construction Completion Year</b>	2027

**Project Scope**

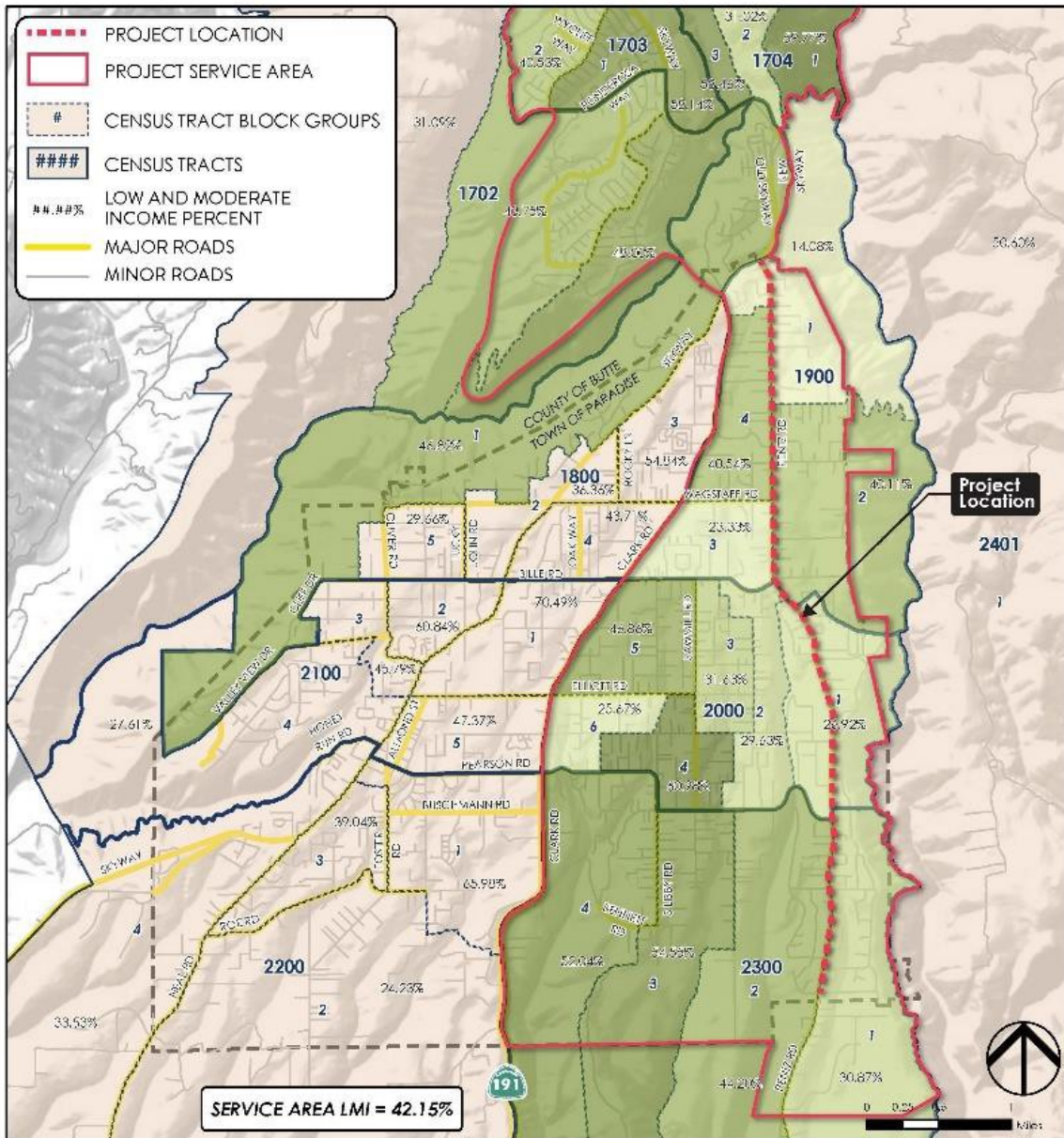
The Pentz Road Widening project will widen Pentz Road from Skyway to the southern Town limits to include a 12-foot-wide center turn lane, widened shoulders, and a multi-use pathway. The project builds upon the FHWA disaster recovery efforts that are rehabilitating the existing roadway pavement.

**Project Description**

Pentz Road serves as a primary north-south ingress/egress route for Town residents. Widening the road will improve evacuation efficiency along this route, provide an adequate shoulder to park disabled vehicles and reduce fire fuel proximity to the traveling public. The project proposes a 10-foot-wide Class I multi-use facility with 2-foot shoulders, lighting, storm drain accommodations, ADA ramps, and crossing surface improvements.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	3
<b>Project Name</b>	Skyway/Pentz Intersection Improvements
<b>Project Type</b>	New Infrastructure Project
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Evacuation Routes
<b>Project Total Cost</b>	\$4,500,000
<b>CDBG-DR Project Grant Amount</b>	\$4,500,000
<b>Environmental Completion Year</b>	2024
<b>Design &amp; ROW Completion Year</b>	2025
<b>Construction Completion Year</b>	2026

**Project Scope**

The Skyway/Pentz Intersection Improvements project will increase capacity with the construction of additional turn lanes and will provide for large vehicle turning radius. The project will require right-of-way acquisition.

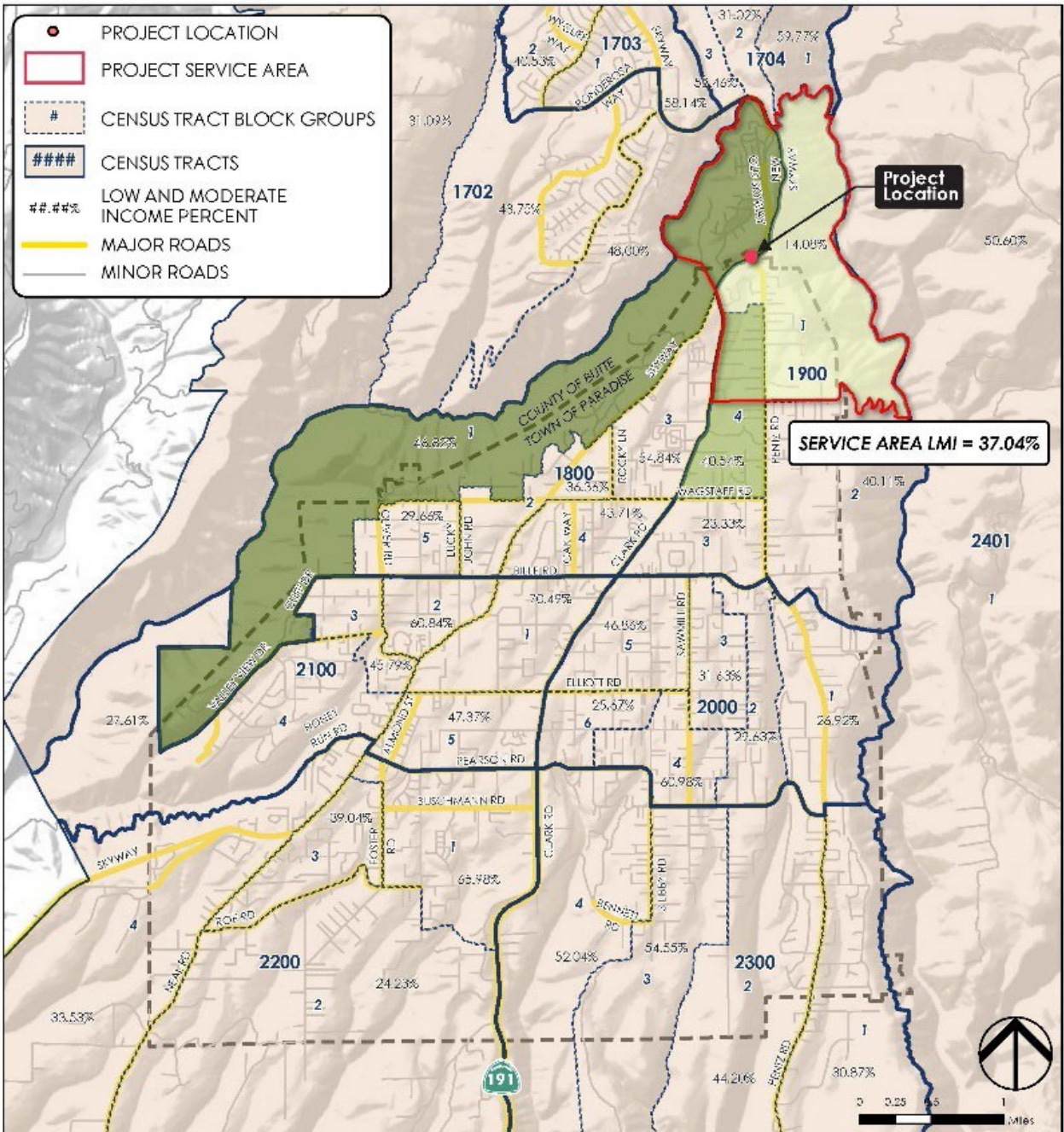
**Project Description**

The Skyway/Pentz Intersection Improvements project will construct additional turn lanes and a traffic signal system or a roundabout at the existing narrow, Two-Way Stop Controlled Skyway/Pentz intersection. Additional capacity and large vehicle turning space is needed at this location for more efficient emergency services. The control type decision will be made early, during the engineering feasibility study phase of the project. The project includes right-of-way acquisition in the intersection vicinity which is required to accommodate the larger intersection footprint that is anticipated.





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**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	3
<b>Project Name</b>	Pentz Pathway Project Phase II
<b>Project Type</b>	New Infrastructure Project
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Interconnected Path System
<b>Project Total Cost</b>	\$23,293,000
<b>CDBG-DR Project Grant Amount</b>	\$1,000,000
<b>Environmental Completion Year</b>	2024
<b>Design &amp; ROW Completion Year</b>	2026
<b>Construction Completion Year</b>	2027

**Project Scope**

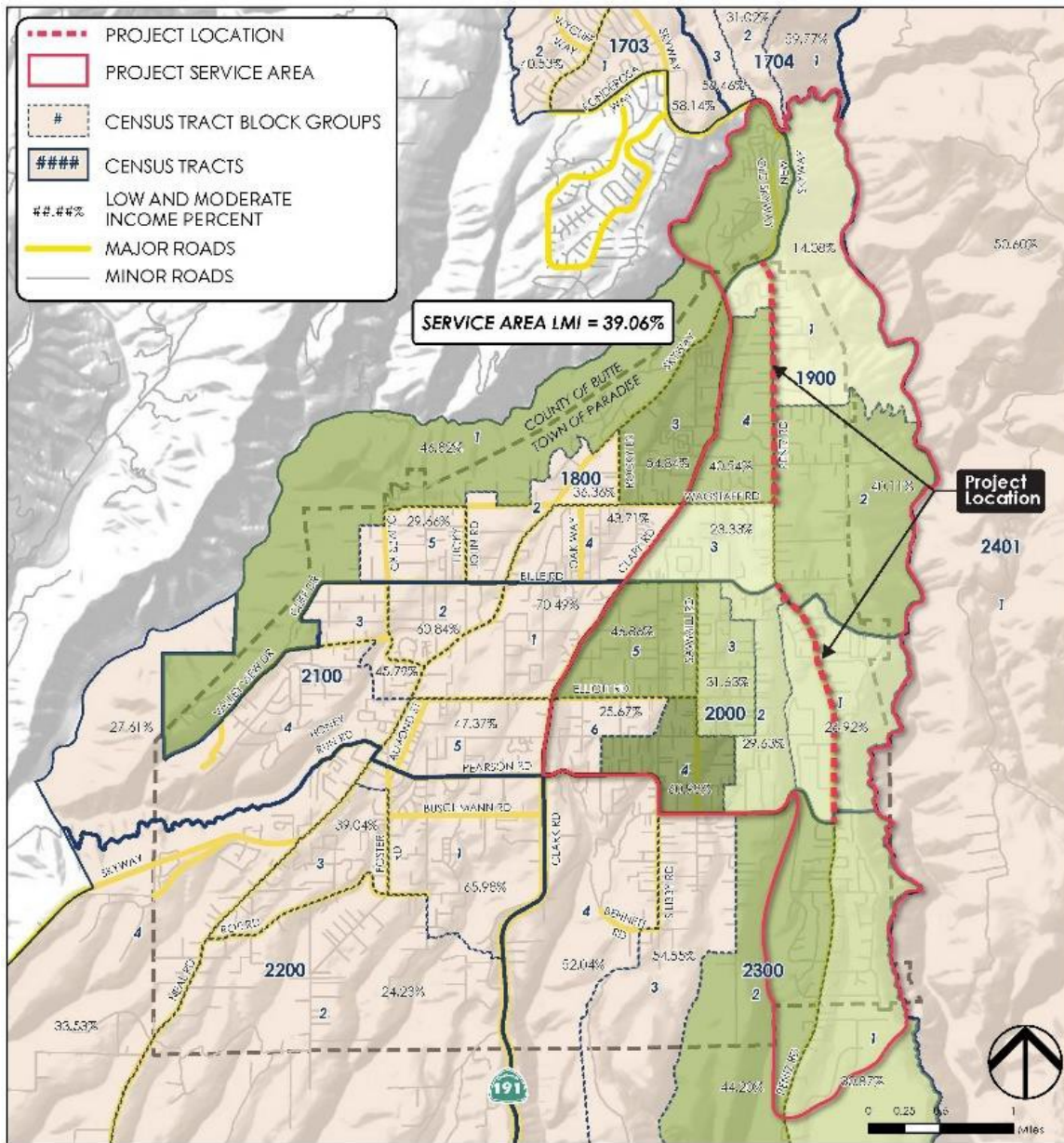
The Pentz Pathway Phase II Project will construct 16,896 linear feet of a 10-foot-wide Class I multi-use facility complete with 2-foot-wide shoulders, lighting, and storm drain accommodations between Skyway to Wagstaff Road and Bille Road to Malibu Drive. Additional improvements include 80 new ADA ramps, 7 new RRFB signals, and 27 crossing surface improvements.

**Project Description**

The proposed project will provide infrastructure improvements for people walking and bicycling in the corridor to reach destinations throughout Town. The project will extend from the recently constructed Pentz Project Phase I north to Skyway, as well as south to Pearson. The project proposes a 10-foot-wide Class I multi-use facility with 2-foot shoulders, lighting, storm drain accommodations, ADA ramps, RRFB's, and crossing surface improvements.



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**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	4
<b>Project Name</b>	Upper Skyway Widening (Billie to Wagstaff)
<b>Project Type</b>	New Infrastructure Project
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Evacuation Routes
<b>Project Total Cost</b>	\$15,000,000
<b>CDBG-DR Project Grant Amount</b>	\$15,000,000
<b>Environmental Completion Year</b>	N/A
<b>Design &amp; ROW Completion Year</b>	N/A
<b>Construction Completion Year</b>	2027

**Project Scope**

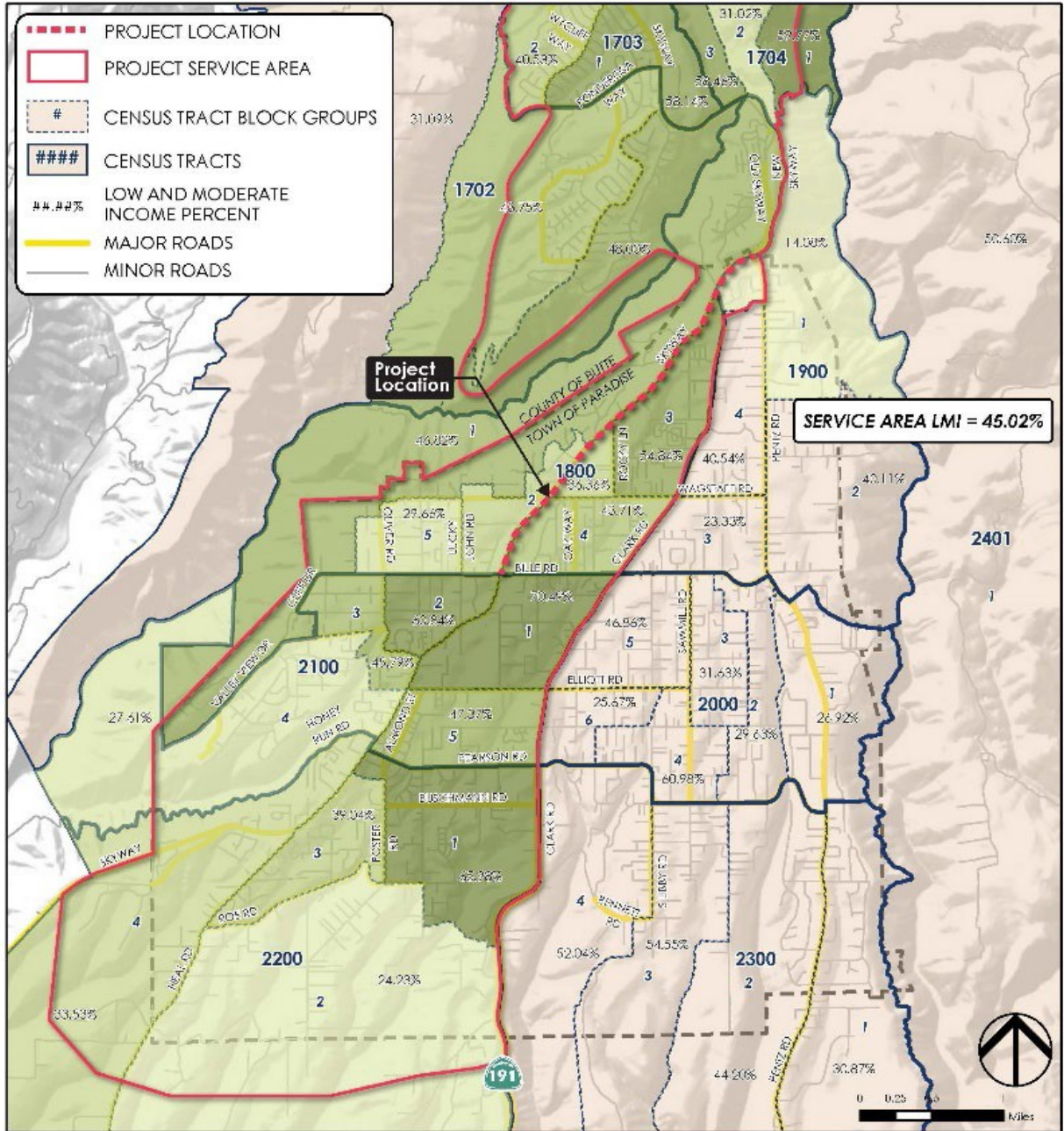
The Upper Skyway Widening project will widen Upper Skyway from Bille Road to Pentz Road (2.7 miles) to include a 12-foot-wide center turn lane, widened shoulders, and a multi-use pathway. The project builds upon the FHWA disaster recovery efforts.

**Project Description**

Skyway serves as the primary ingress/egress route for Town residents. Widening the road will improve evacuation along this route, provide an adequate shoulder to park disabled vehicles and reduce fire fuel proximity to the traveling public. The project includes a grade separated multi-use bike and pedestrian pathway create an interconnected path system. This pathway can function as an emergency ingress/egress during future evacuation events and provide an additional fire break.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	4
<b>Project Name</b>	Skyway Connectivity Project (Bille to Wagstaff)
<b>Project Type</b>	New Infrastructure project
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Interconnected Path System
<b>Project Total Cost</b>	\$6,810,000
<b>CDBG-DR Project Grant Amount</b>	\$250,000
<b>Environmental Completion Year</b>	2024
<b>Design &amp; ROW Completion Year</b>	2025
<b>Construction Completion Year</b>	2026

**Project Scope**

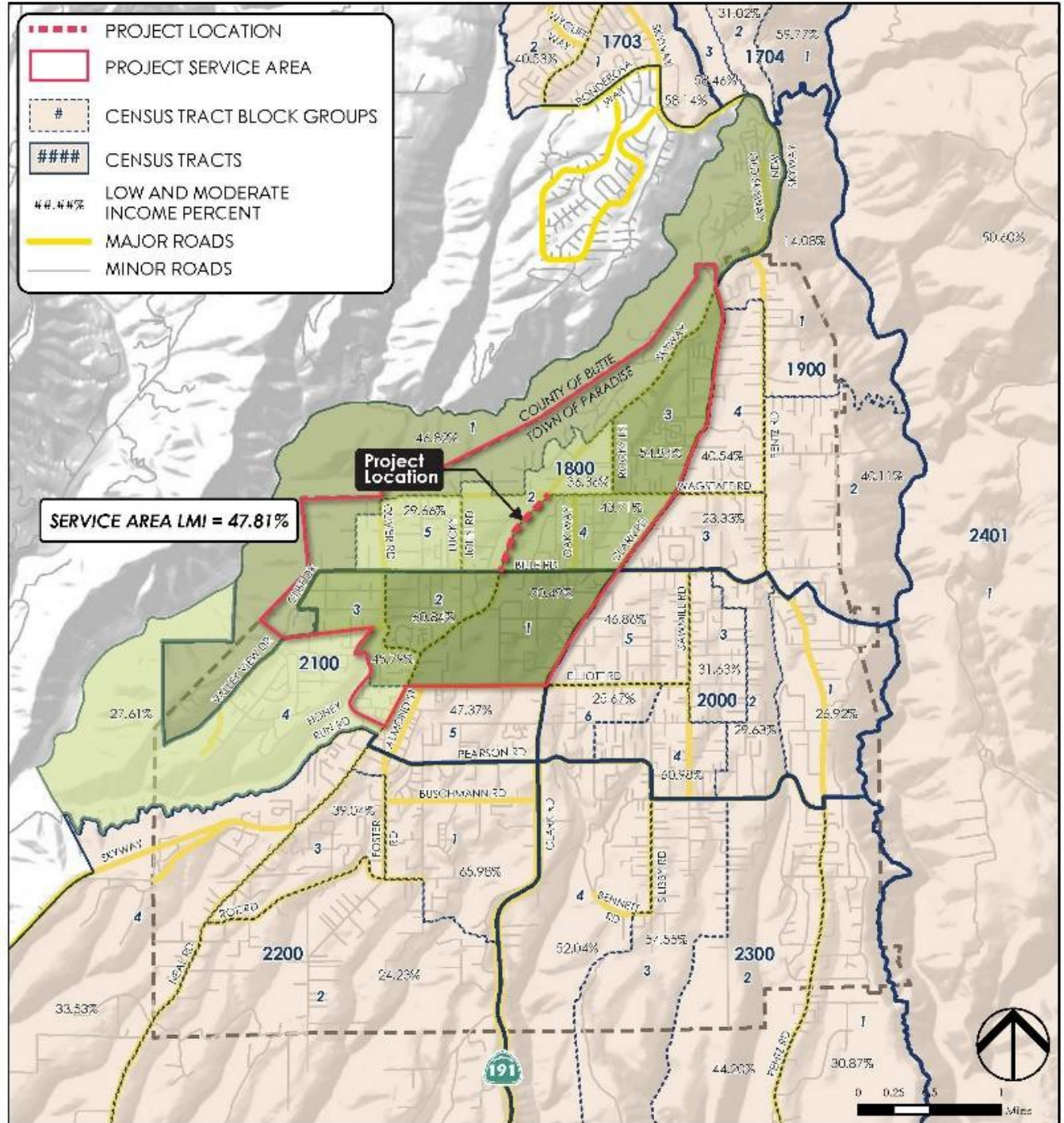
The Skyway Connectivity Project will construct 5 segments of sidewalk infill totaling 4,255 linear feet, 20 new ADA ramps, and stripe 3,165 linear feet of on-street bicycle lanes between Wagstaff Road and Bille Road. The project also constructs 20 ADA ramps and undergrounds overhead utilities.

**Project Description**

The proposed project will provide infrastructure improvements for people walking and bicycling in the corridor to reach destinations throughout Town. The proposed improvements serve a dual purpose and provide increased capacity for citizens as well as ingress/egress for emergency response vehicles during evacuation events.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	5
<b>Project Name</b>	Paradise ATP Gateway Project (Neal Road Class I)
<b>Project Type</b>	New Infrastructure Project
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Interconnected Path System
<b>Project Total Cost</b>	\$13,068,000
<b>CDBG-DR Project Grant Amount</b>	\$800,000
<b>Environmental Completion Year</b>	2024
<b>Design &amp; ROW Completion Year</b>	2025
<b>Construction Completion Year</b>	2026

**Project Scope**

The Paradise Gateway Project will construct an 8,554-foot long, grade-separated, Class I multi-use facility along Neal Road and 4,752 linear feet of sidewalk infill along Skyway between Neal Road and Pearson Road. The project also constructs 50 ADA ramps, up to 3 Rectangular Rapid Flashing Beacons (RRFBs), and undergrounds overhead utilities.

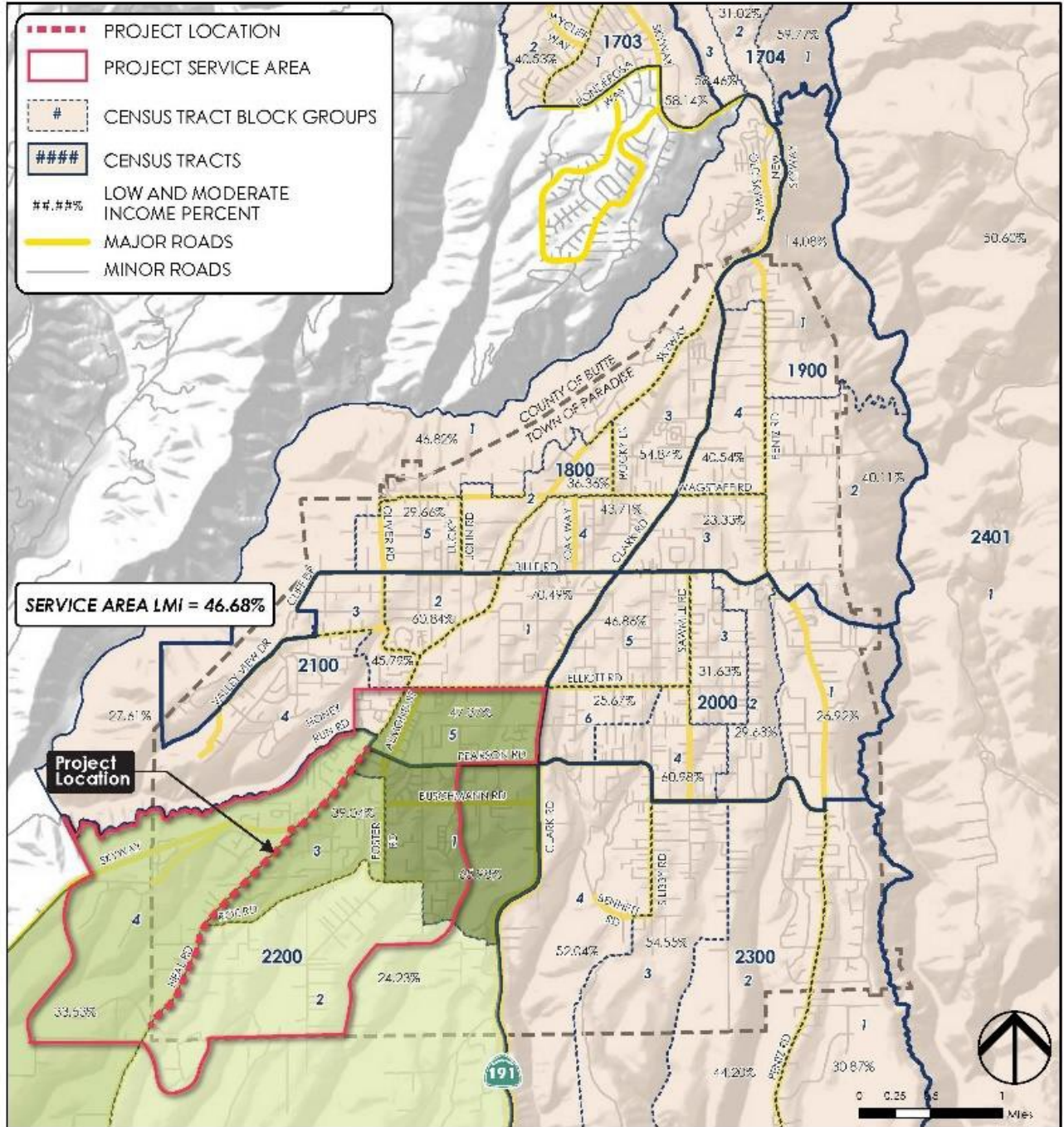
**Project Description**

The proposed project will provide infrastructure improvements for people walking and bicycling in the corridor to reach destinations throughout Town. The project will provide an 8,554-foot grade-separated, Class I multi-use facility along Neal Road, 50 ADA ramps, up to 3 RRFBs, and 4,752 linear feet of sidewalk infill along Skyway between Neal Road and Pearson Road.





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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	6
<b>Project Name</b>	Elliott/Nunneley Road Extension
<b>Project Type</b>	New Infrastructure Project
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Missing Road Segment
<b>Project Total Cost</b>	\$7,500,000
<b>CDBG-DR Project Grant Amount</b>	\$7,500,000
<b>Environmental Completion Year</b>	2024
<b>Design &amp; ROW Completion Year</b>	2025
<b>Construction Completion Year</b>	2026

**Project Scope**

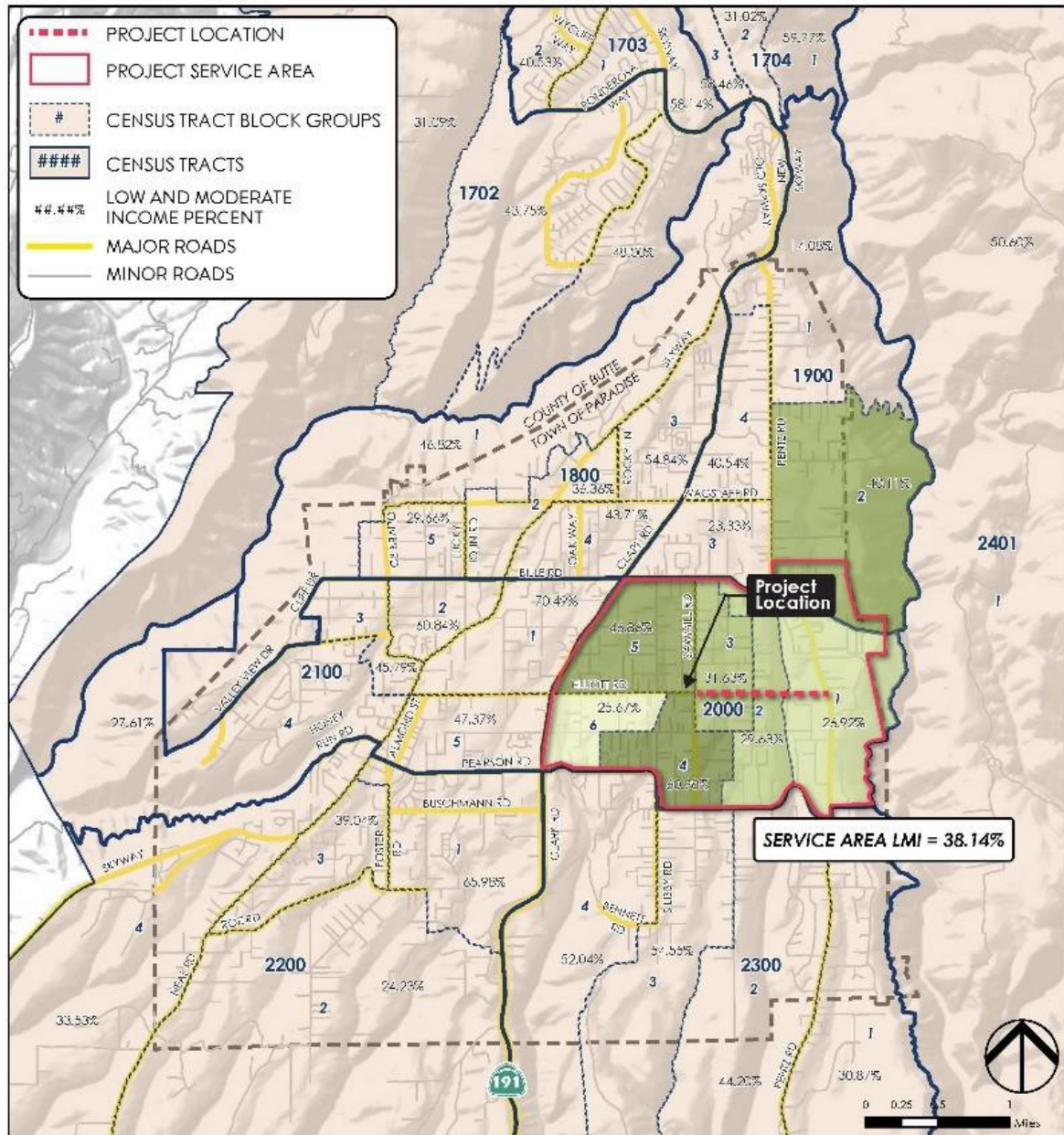
The Elliott Road Extension Project will extend the existing roadway 0.7-miles from its easterly terminus to Pentz Road. The extension will include a two-lane roadway with sidewalks, bike lanes, and lighting.

**Project Description**

The project will extend Elliott Road 0.7-miles east to Pentz Road and provide alternative ingress/egress for the neighborhood as well as interconnectivity between primary evacuation routes within Town. The project also supports daily multimodal access and circulation needs of the community. The project improves access to evacuation routes, completes missing road segments, reduces dead-end streets, reduces fire fuel proximity to the roadway, and is consistent with Town Long-Term Recovery Plan.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	7
<b>Project Name</b>	Oliver Curve Pathway Phase I
<b>Project Type</b>	New Infrastructure Project
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Interconnected Path System
<b>Project Total Cost</b>	\$13,100,000
<b>CDBG-DR Project Grant Amount</b>	\$3,100,000
<b>Environmental Completion Year</b>	2024
<b>Design &amp; ROW Completion Year</b>	2025
<b>Construction Completion Year</b>	2027

**Project Scope**

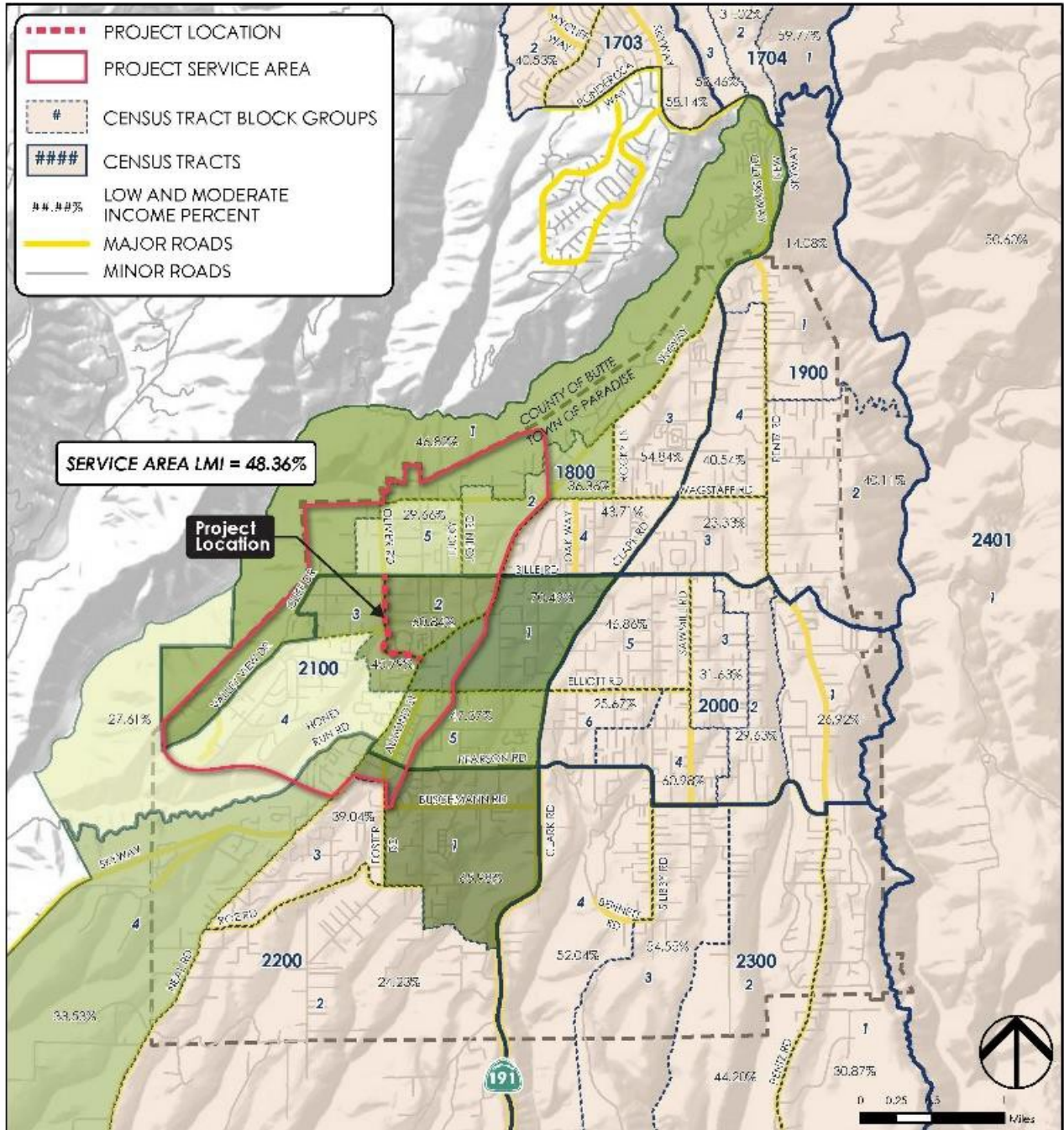
The Oliver Curve Pathway Phase I project will construct 3,696 linear feet of a 10-foot-wide Class I multi-use facility complete with 2-foot-wide shoulders, lighting, and storm drain accommodations. Additional improvements include 17 new ADA ramps, 1 new crosswalk, and retaining walls.

**Project Description**

The Oliver Curve Pathway Project Phase I proposes a Class I pathway on Oliver Road, stretching three-quarters of a mile from Skyway to Bille Road. The multi-use facility is 10 feet wide with 2-foot shoulders and includes 17 ADA ramps, pathway lighting, and retaining walls along Honey Run Creek. The project will provide improved access for people walking and bicycling to access destinations throughout Town including improved connectivity to the Yellowstone Kelly Heritage Trail.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	7
<b>Project Name</b>	Foster/Black Olive Intersection Improvements
<b>Project Type</b>	New Infrastructure Project
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Evacuation Routes
<b>Project Total Cost</b>	\$3,500,000
<b>CDBG-DR Project Grant Amount</b>	\$3,500,000
<b>Environmental Completion Year</b>	2024
<b>Design &amp; ROW Completion Year</b>	2025
<b>Construction Completion Year</b>	2026

**Project Scope**

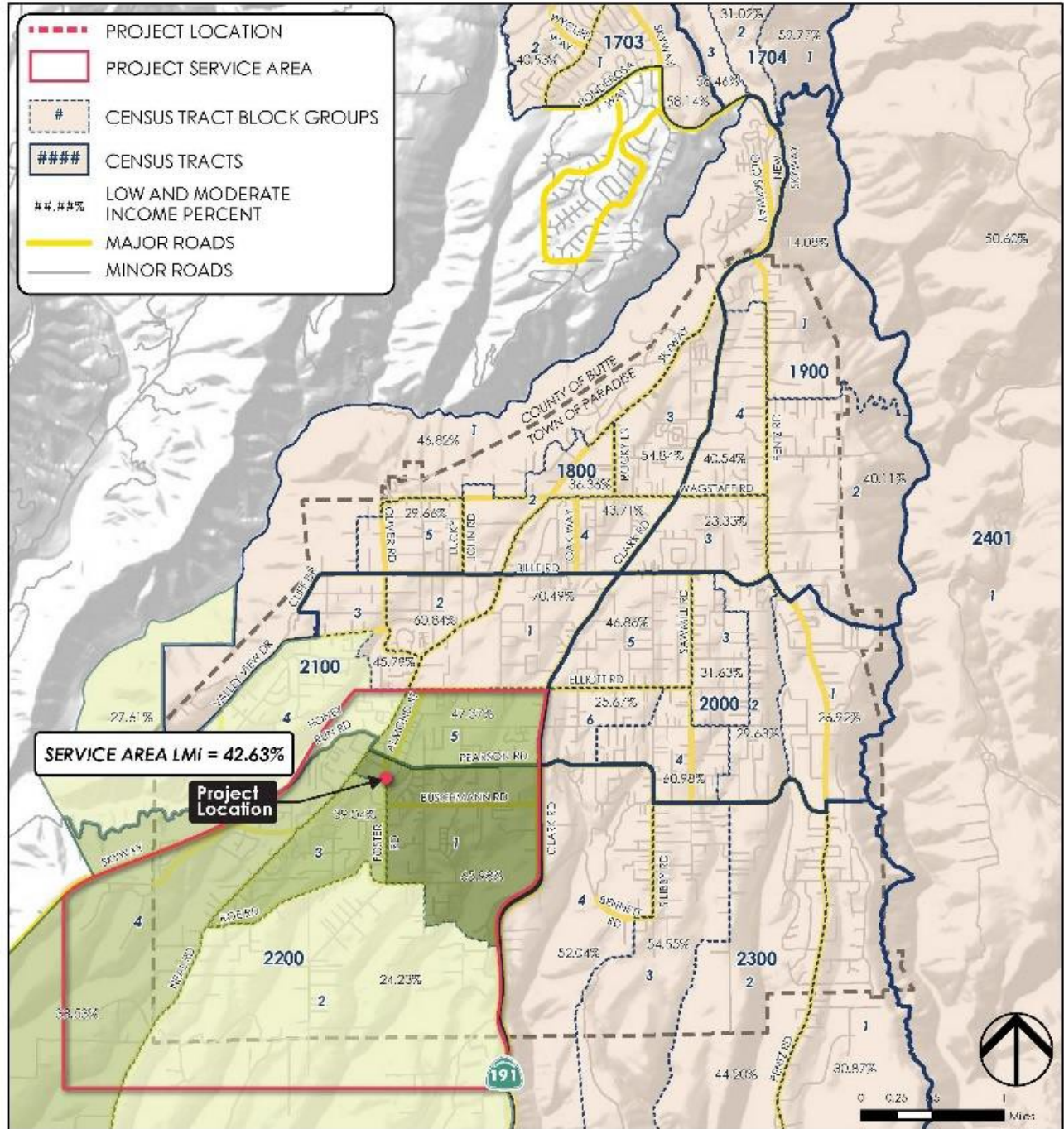
The Black Olive/Foster Intersection Improvements project will include roadway realignment, the construction of turn lanes, and the installation of a traffic signal or a roundabout.

**Project Description**

The Black Olive/Foster Intersection Improvements project will realign Black Olive Drive both horizontally and vertically through the Foster Drive intersection, construct additional turn lanes, and revise the intersection controls. The project includes constructing either a traffic signal system or roundabout. The intersection control decision will be made during the engineering feasibility study (early phase) of the project. The project includes right-of-way acquisition in the intersection vicinity which will be required to realign the roadway in accordance with nationally recognized design guidelines.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	9
<b>Project Name</b>	Forest Service Road Improvements
<b>Project Type</b>	New Infrastructure Project
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Missing road Segment
<b>Project Total Cost</b>	\$3,000,000
<b>CDBG-DR Project Grant Amount</b>	\$3,000,000
<b>Environmental Completion Year</b>	2024
<b>Design &amp; ROW Completion Year</b>	2025
<b>Construction Completion Year</b>	2026

**Project Scope**

The project acquires the right of way and improves approximately 1,940 feet of Forest Service Road by removing a layer of existing pavement via Cold Plane (CP) and resurfacing with hot mix asphalt overlay (HMA). In addition, the project extends the road from its current east-west alignment 900 feet west to connect to Skyway. Improvements include paving, striping, new shoulders, bike lane, and drainage facilities.

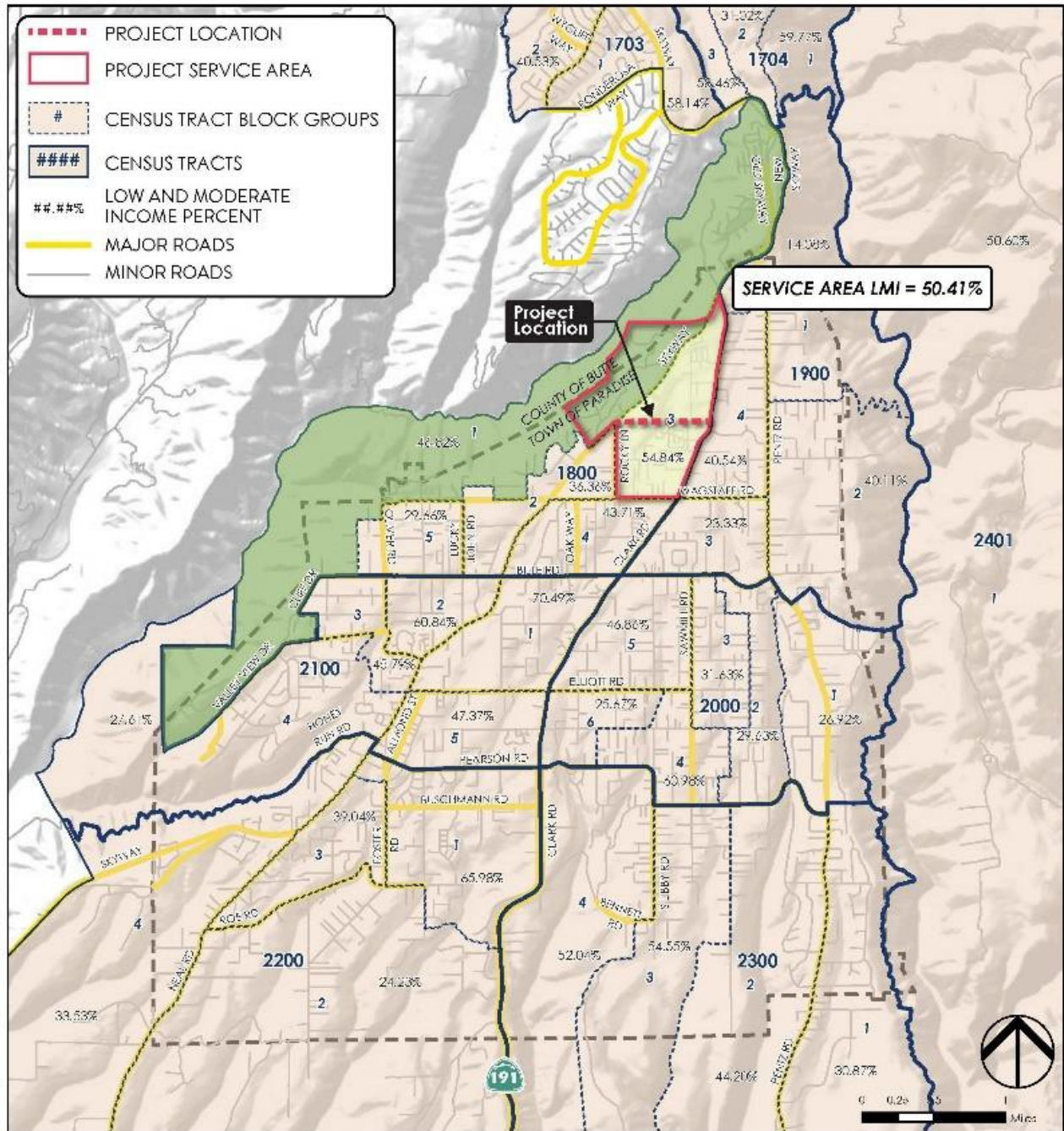
**Project Description**

The project improves and extends Forest Service Road, an east-west route connecting Clark Road to a public roadway. Butte County Fire Station 35 is at the corner of Forest Service Road and Moore Road. The improved roadway will provide a missing connection between Skyway and Clark Road, primary evacuation routes for the community.





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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	M1
<b>Project Name</b>	On-System Road Rehabilitation
<b>Project Type</b>	Match – FHWA ER
<b>HUD National Objective</b>	Unmet Need/Low Moderate Income
<b>Long-Term Recovery Plan</b>	Evacuation Routes
<b>Project Total Cost</b>	\$55,439,200
<b>CDBG-DR Project Grant Amount</b>	\$13,800,000
<b>Environmental Completion Year</b>	Complete
<b>Design &amp; ROW Completion Year</b>	Complete
<b>Construction Completion Year</b>	2027

**Project Scope**

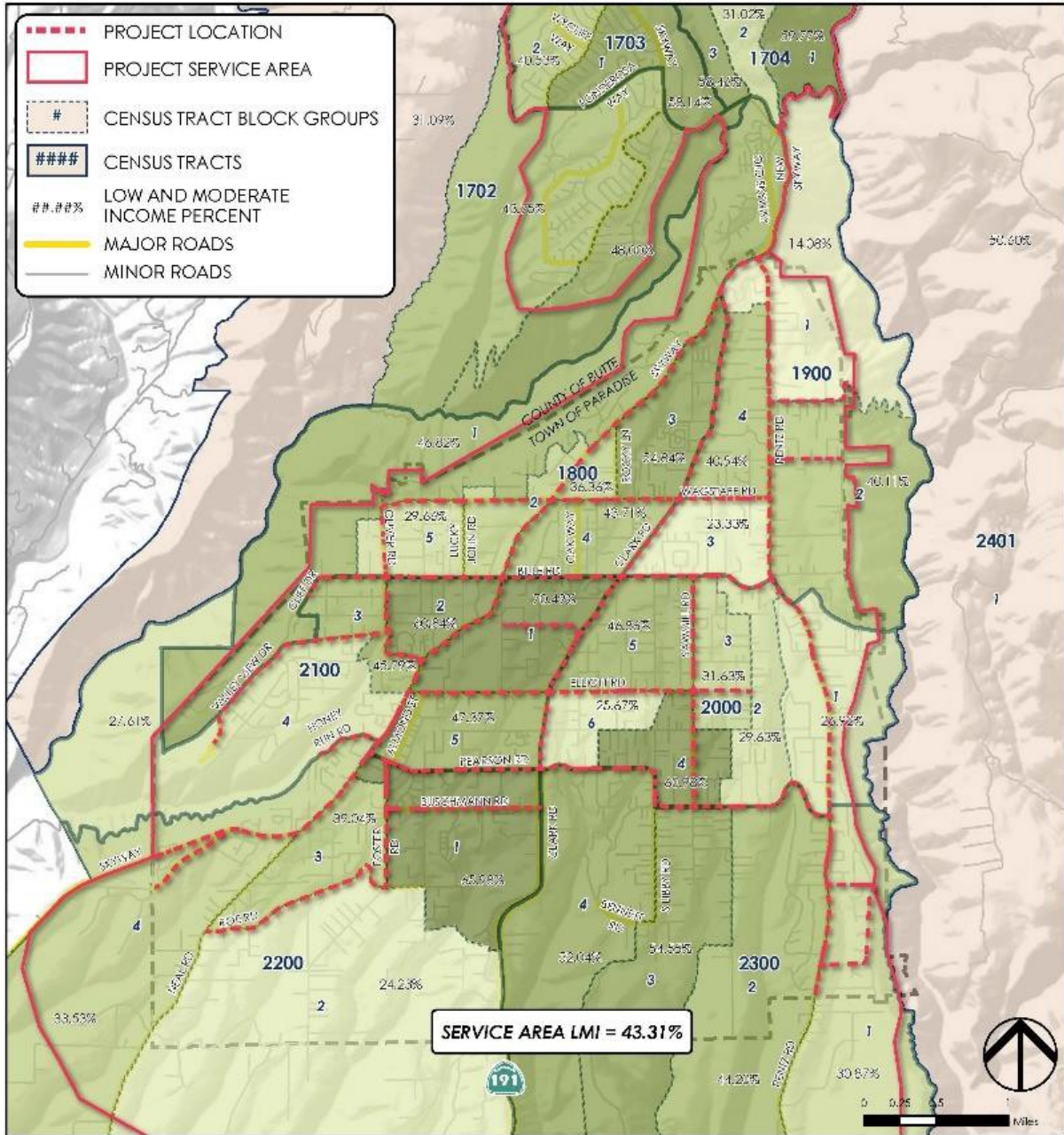
The On-System Road Rehabilitation Project repairs damaged sections of roadway and re-striping when necessary to rehabilitate 32 miles of roadway segments of on-system roadway. The project includes a combination of pavement rehabilitation strategies depending on the severity of damage, modifications to nine signalized intersections, and construction of 48 ADA compliant curb ramps.

**Project Description**

The project will repair 32 miles of on-system roadways that sustained heavy damage in the wake of the Camp Fire and debris removal efforts. The critical condition of the current roadways risks pavement failure and creates unsafe road conditions. The project will improve safety along public roads that are part of the federal-aid system. These arterial roadways are either evacuation routes or primary east-west connections to the evacuation routes.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	M2
<b>Project Name</b>	Off-System Road Rehabilitation
<b>Project Type</b>	Match – FEMA PA
<b>HUD National Objective</b>	Unmet Need/ Low Moderate Income
<b>Long-Term Recovery Plan</b>	Evacuation Routes
<b>Project Total Cost</b>	\$45,043,700
<b>CDBG-DR Project Grant Amount</b>	\$7,308,635
<b>Environmental Completion Year</b>	Complete
<b>Design &amp; ROW Completion Year</b>	Complete
<b>Construction Completion Year</b>	2026

**Project Scope**

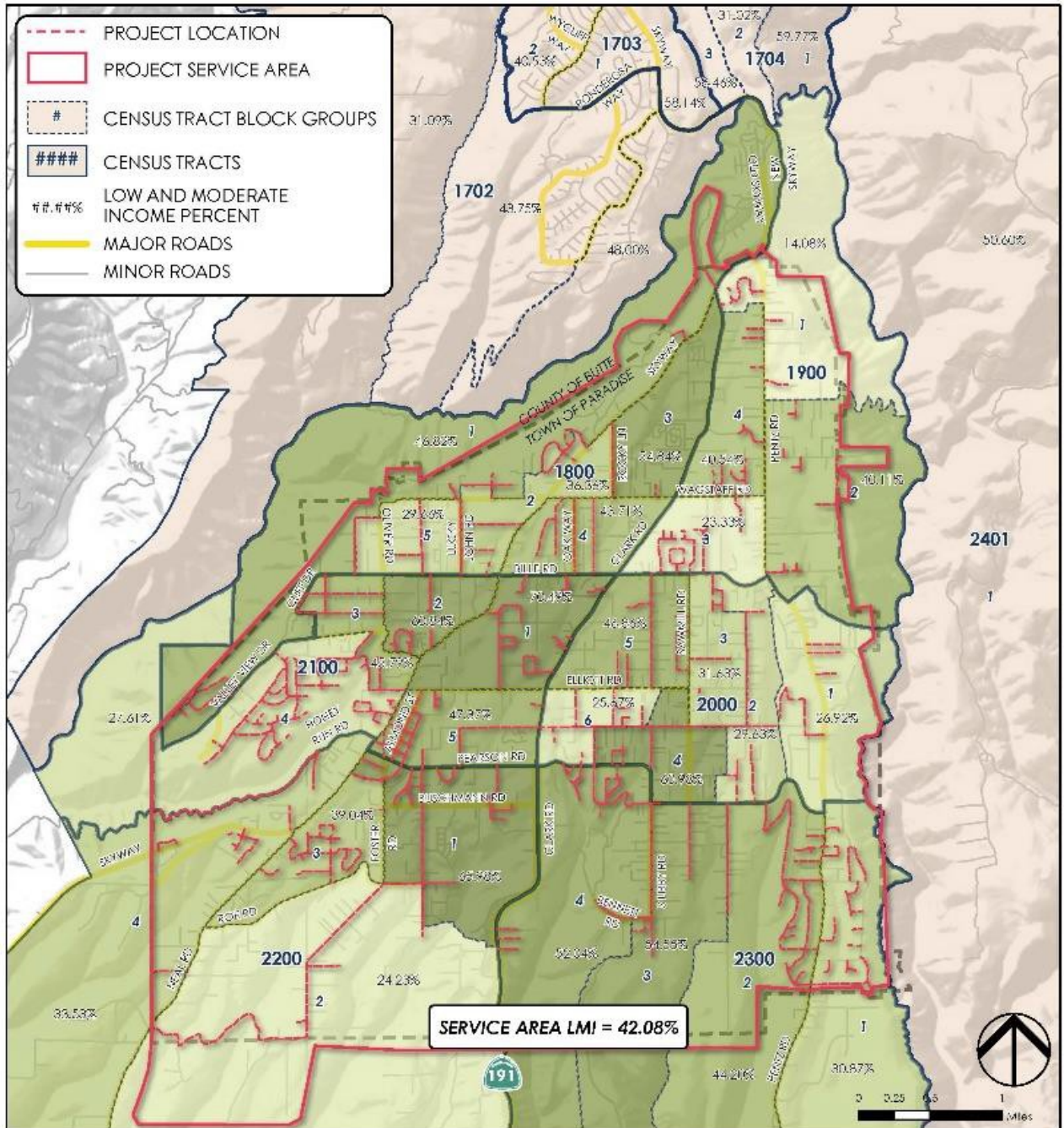
The Off-System Road Rehabilitation Project repairs damaged sections of roadway and re-striping when necessary to rehabilitate 47 miles of roadway segments of on-system roadway. The project also upgrades non-compliant ADA curb ramps.

**Project Description**

The project will repair 47 miles of off-system roadways that sustained heavy damage in the wake of the Camp Fire. The critical condition of the current roadways risks pavement failure and creates unsafe road conditions. The project will improve safety along public roads that are not part of the federal-aid system. These roads include local roads and rural minor collectors, which provide evacuation routes connecting to the federal-aid system.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	M3
<b>Project Name</b>	Early Warning System
<b>Project Type</b>	Match – FEMA HMGP
<b>HUD National Objective</b>	Unmet Need/ Low Moderate Income
<b>Long-Term Recovery Plan</b>	Emergency Notification
<b>Project Total Cost</b>	\$3,207,400
<b>CDBG-DR Project Grant Amount</b>	\$756,000
<b>Environmental Completion Year</b>	Complete
<b>Design &amp; ROW Completion Year</b>	Complete
<b>Construction Completion Year</b>	2023

**Project Scope**

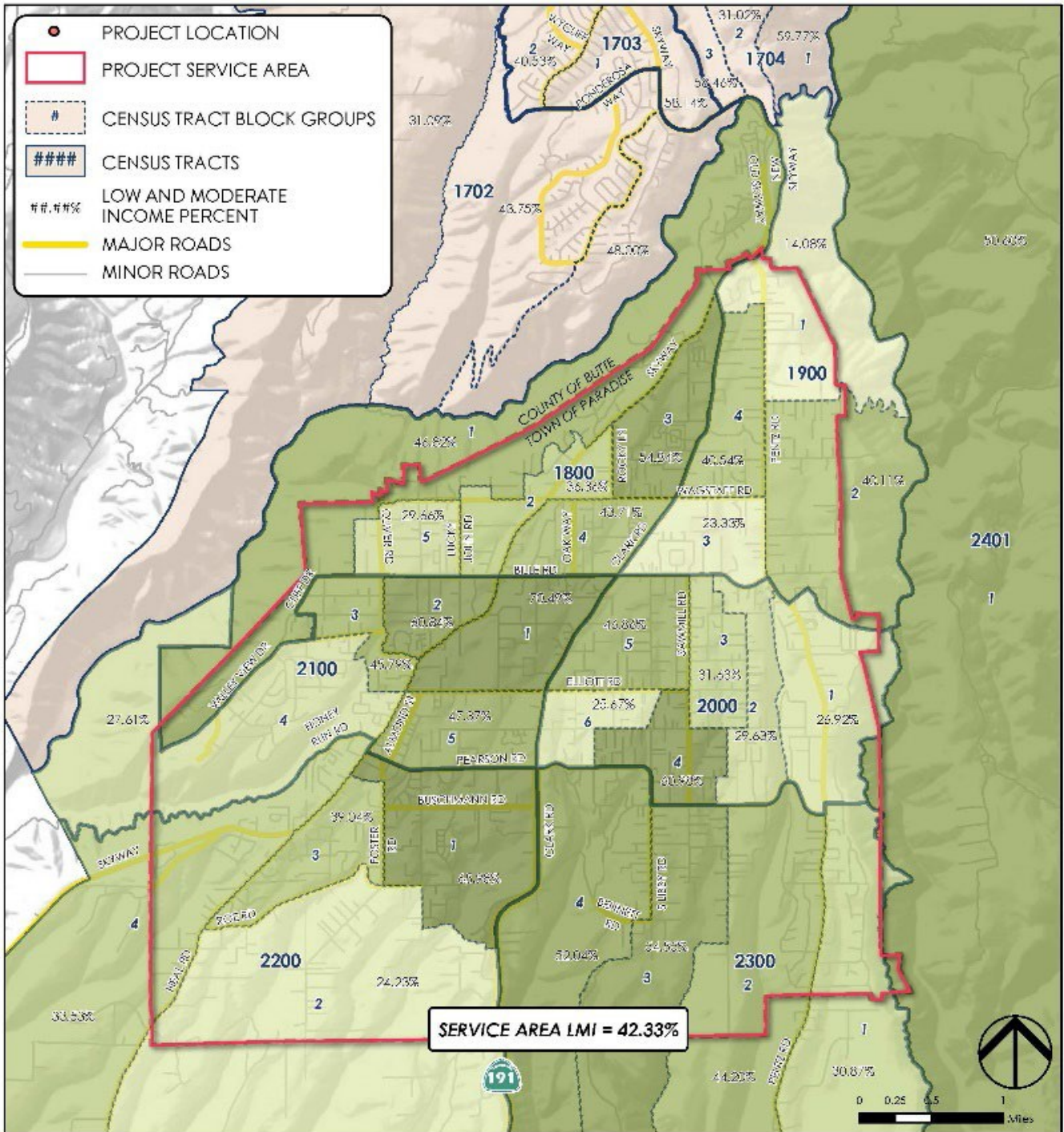
The Early Warning System project will design, install, and implement a network of emergency warning systems for the purpose of rapid hazard notification.

**Project Description**

This project will provide an early warning system with multiple notification channels, including IPAWS/WEA, AM Radio, SMS, social media, and outdoor siren/speakers capable of broadcasting audible alert tones and clear voice messages. The siren/speaker system will be installed in 21 locations throughout Paradise. The system is intended to have battery backup, AC, solar power, user friendly cloud-based software, and a reliable connection that is not reliant on local power or cell towers.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	M4
<b>Project Name</b>	Hazardous Fuel Reduction Program
<b>Project Type</b>	Match – FEMA HMGP
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Fuels Management
<b>Project Total Cost</b>	\$8,465,986
<b>CDBG-DR Project Grant Amount</b>	\$475,000
<b>Environmental Completion Year</b>	2023
<b>Design &amp; ROW Completion Year</b>	2023
<b>Construction Completion Year</b>	2026

**Project Scope**

The Defensible Space Code Enforcement project will remove hazardous fuel in the wake of the Camp Fire within Town.

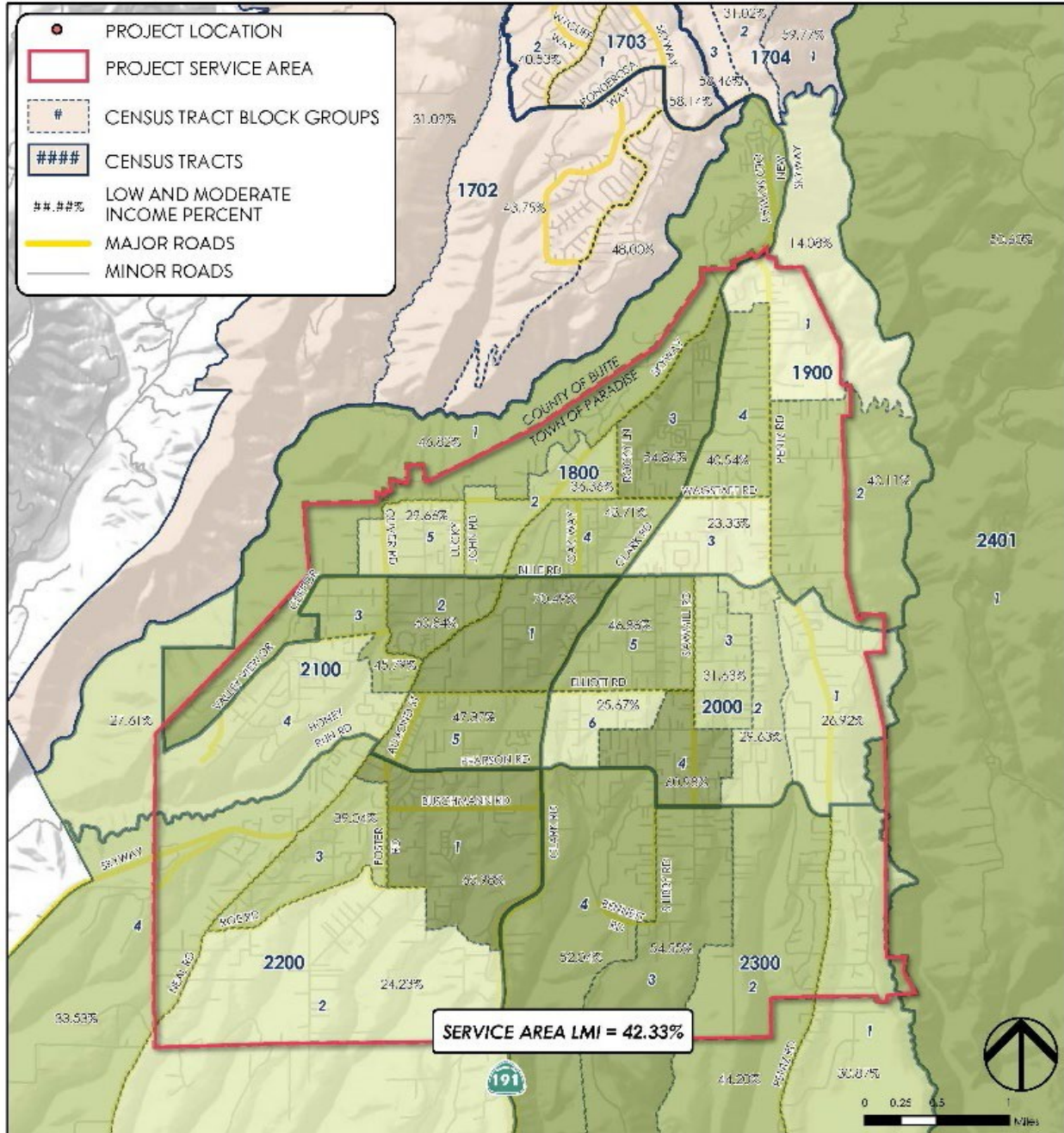
**Project Description**

The project will dispose of most vegetative debris at the Green Waste Yard adjacent to the Public Works yard on American Way in Paradise.





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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

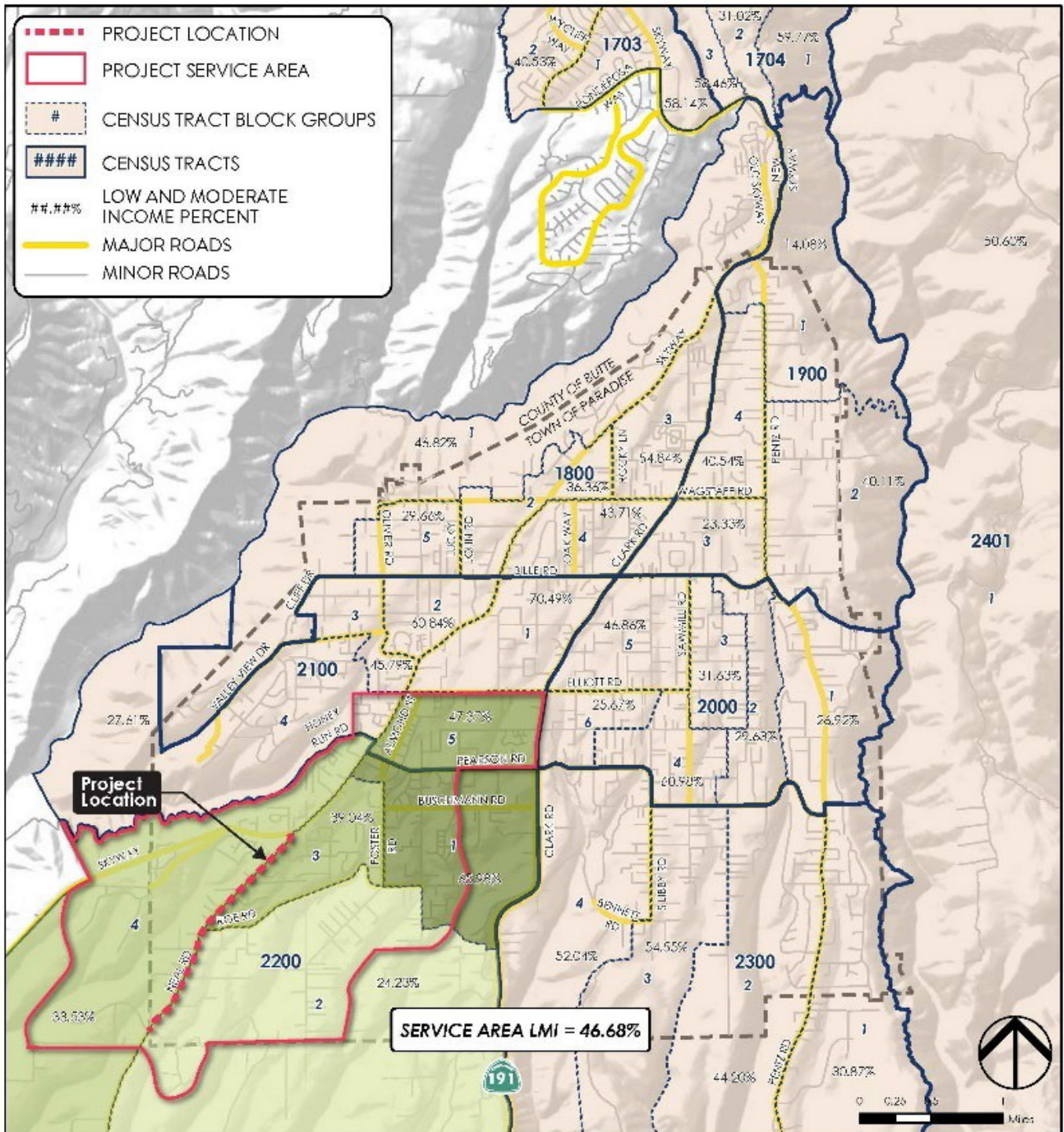
<b>Allocation Priority</b>	M5
<b>Project Name</b>	Neal Road Rehabilitation
<b>Project Type</b>	Match – FHWA ER
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Evacuation Routes
<b>Project Total Cost</b>	\$1,588,300
<b>CDBG-DR Project Grant Amount</b>	\$425,000
<b>Environmental Completion Year</b>	Complete
<b>Design &amp; ROW Completion Year</b>	Complete
<b>Construction Completion Year</b>	2026

**Project Scope**

The project rehabilitates approximately 1.63 miles of Neal Road within the Town limits between Skyway and Wayland. The project work includes 1-inch Cold Plane and 3-inch asphalt concrete (AC) overlay for the entire roadway section with digout areas of 12-inch asphalt AC for sections with severe rutting and cracking damage.

**Project Description**

Neal Road is an on-system, principal arterial, major connector route in from the Town of Paradise to State Route 99 (SR 99) and it is designated as a federal aid route within the Town Limits. Neal Road is one of the primary routes between the Neal Road Landfill just east of SR 99 and the Town. The rehabilitation consists of asphalt concrete overlays and structural section repair (digouts) for severely damaged locations.





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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	M6
<b>Project Name</b>	On-System Culvert Replacement
<b>Project Type</b>	Match- FHWA ER
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Evacuation Routes
<b>Project Total Cost</b>	\$811,000
<b>CDBG-DR Project Grant Amount</b>	\$100,000
<b>Environmental Completion Year</b>	Complete
<b>Design &amp; ROW Completion Year</b>	Complete
<b>Construction Completion Year</b>	2023

**Project Scope**

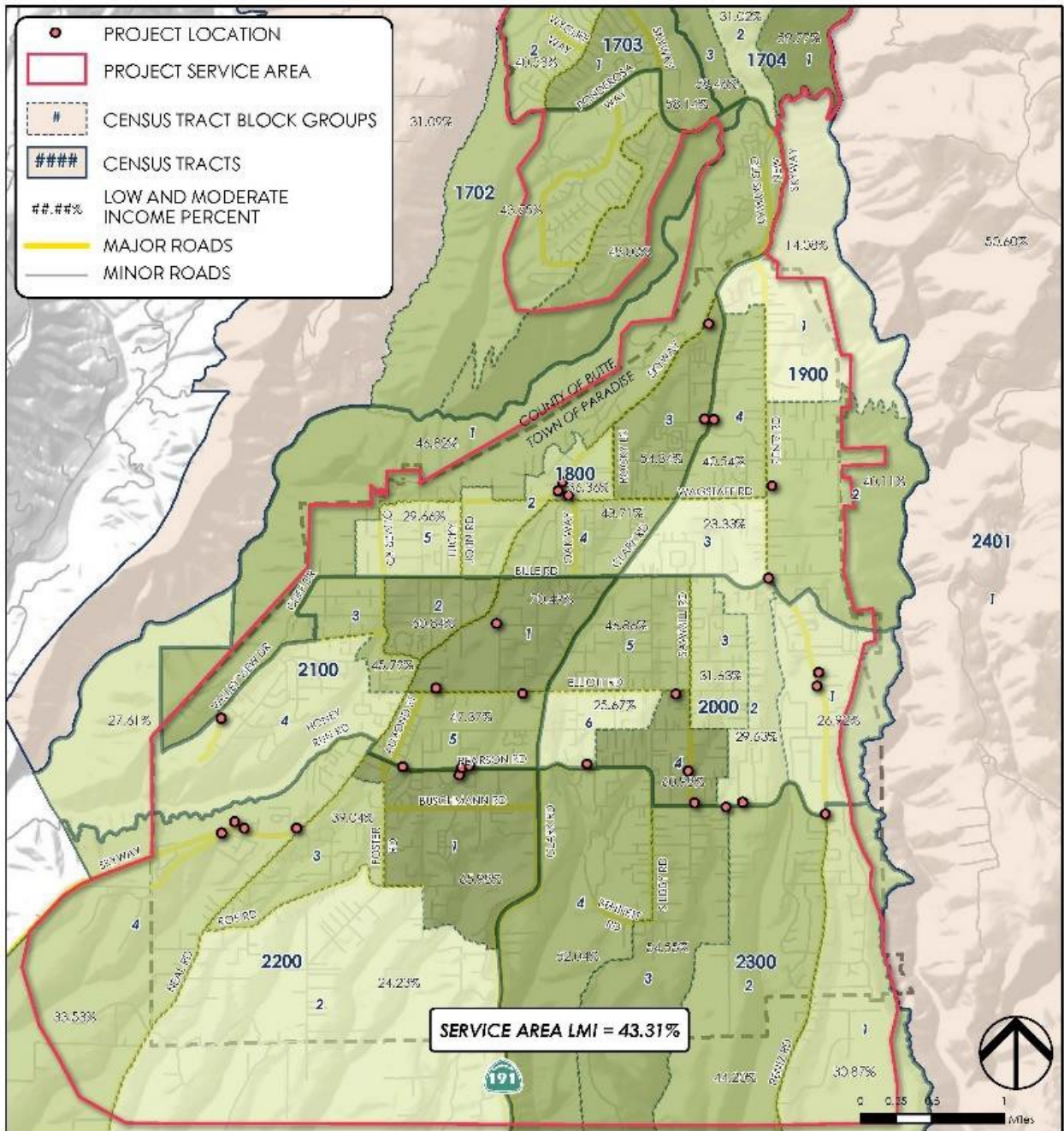
The On-System Culvert Replacement Project involves replacing damaged On-System High Density Poly Ethylene (HDPE) “plastic” culverts with Reinforced Concrete Pipe (RCP) culverts, including restoration of the roadway section above the pipe at various locations. The project is funded through FHWA Emergency Relief.

**Project Description**

The project will repair damaged culverts at 25 locations along evacuation routes to improve safety and provide safe and effective management of storm runoff. Consistent with the Long-Term Recovery Plan, the project provides for a more sustainable and greener community.



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**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	M7
<b>Project Name</b>	On- System Hardscape Replacement
<b>Project Type</b>	Match- FHWA ER
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Walkable Downtown
<b>Project Total Cost</b>	\$767,100
<b>CDBG-DR Project Grant Amount</b>	\$75,000
<b>Environmental Completion Year</b>	Complete
<b>Design &amp; ROW Completion Year</b>	Complete
<b>Construction Completion Year</b>	2023

**Project Scope**

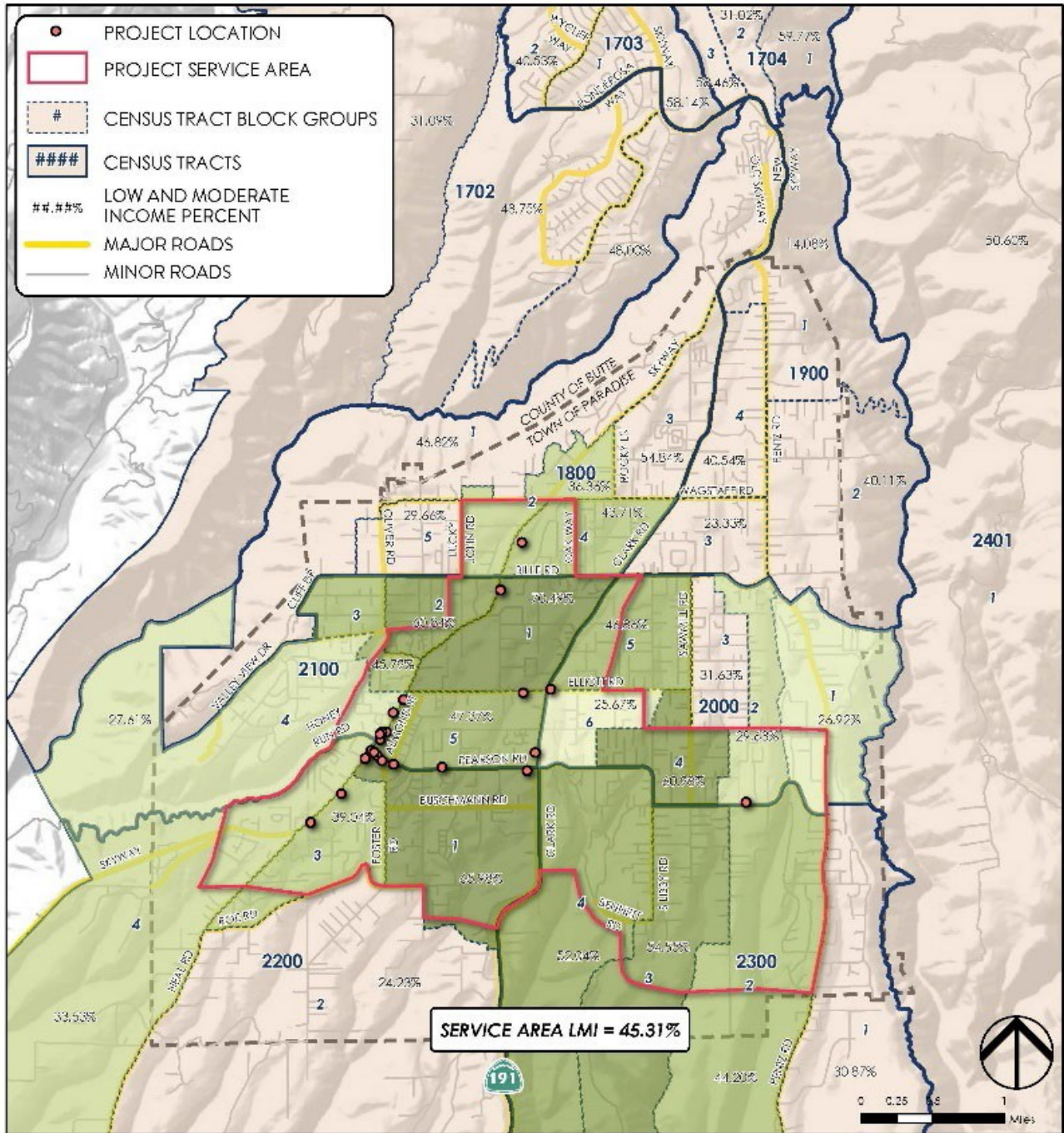
The On-System Hardscape Replacement Project involves removal and replacement of damaged concrete curb, gutter, and sidewalk at 22 locations along on-system roads that are part of the federal-aid system. The project is partially funded through FHWA Emergency Relief.

**Project Description**

The project will repair damaged hardscape along on-system roads to improve safety. The Town's hardscape sustained heavy damage in the wake of the Camp fire. Damaged areas pose a hazard to pedestrians and bicyclists. Consistent with the Long-Term Recovery Plan, the project repairs will support an interconnected path system and walkable downtown.



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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	M8
<b>Project Name</b>	Off-System Culvert Repair
<b>Project Type</b>	Match – FEMA PA
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Evacuation Routes
<b>Project Total Cost</b>	\$750,000
<b>CDBG-DR Project Grant Amount</b>	\$50,000
<b>Environmental Completion Year</b>	Complete
<b>Design &amp; ROW Completion Year</b>	Complete
<b>Construction Completion Year</b>	2023

**Project Scope**

The Off-System Culvert Replacement Project involves replacing damaged On-System High Density Poly Ethylene (HDPE) culverts and rehabilitation of the roadway section above the pipe at 31 locations.

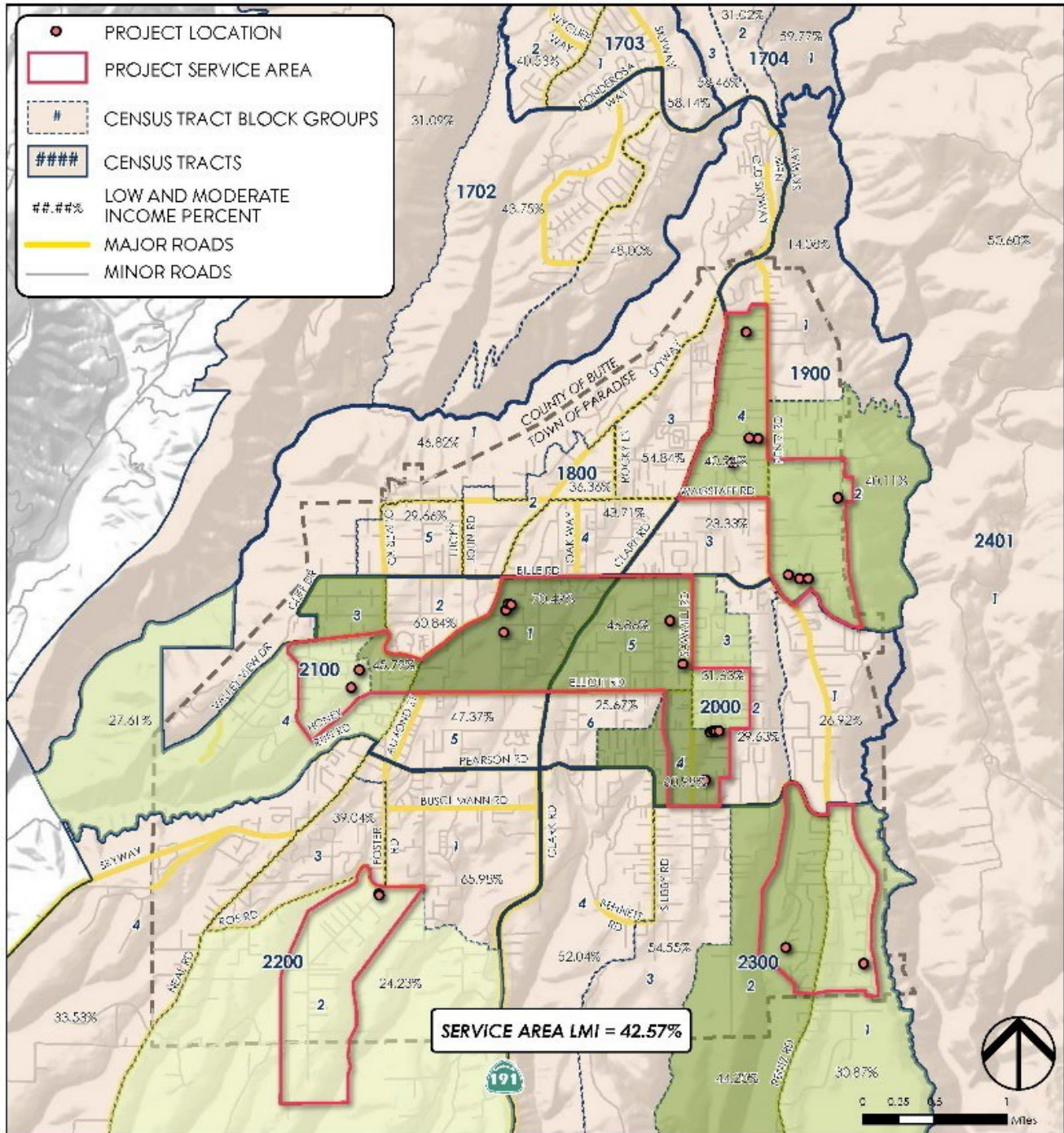
**Project Description**

The project will repair damaged culverts at 31 locations along neighborhood roads connecting to evacuation routes to improve safety and provide safe and effective management of storm water runoff. The culverts targeted for inspection were identified by using the Town’s Acela inventory database. Plastic culverts that were listed as off the federal and state system and located within the burn areas were identified.





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**Town of Paradise**  
**Community Development Block Grant Disaster Recovery – Infrastructure**  
**Project Fact Sheet**

<b>Allocation Priority</b>	M9
<b>Project Name</b>	On-System Sign Replacement
<b>Project Type</b>	Match – FHWA ER
<b>HUD National Objective</b>	Unmet Need
<b>Long-Term Recovery Plan</b>	Evacuation Routes
<b>Project Total Cost</b>	\$285,200
<b>CDBG-DR Project Grant Amount</b>	\$35,000
<b>Environmental Completion Year</b>	Complete
<b>Design &amp; ROW Completion Year</b>	Complete
<b>Construction Completion Year</b>	2023

**Project Scope**

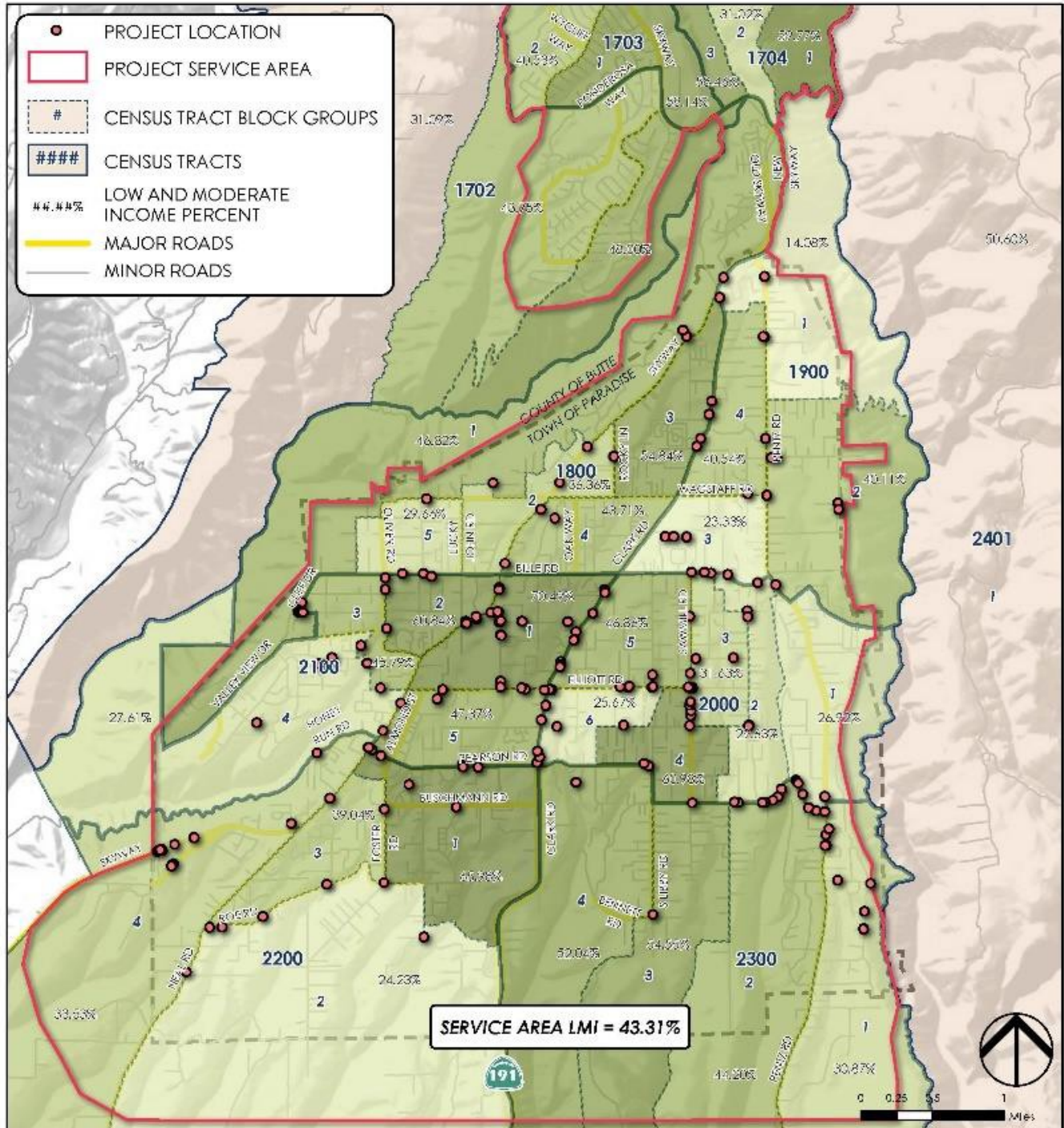
The On-System Sign Replacement Project involves sign removal, sign installation and reset of roadside signs and flashing beacons at 122 locations damaged in the 2018 Camp Fire. The project is partially funded through FHWA Emergency Relief.

**Project Description**

The project will replace damaged roadway signs along federal aid routes to improve safety. Signage types include stop, no parking, signs alerting to fire station access, speed limit, curves ahead, street markers, prepare to stop/stop ahead, merge left, school crossing, flashing beacons, hospital/police, no outlet, wildlife crossing, etc.



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